



28th April 2020

Addendum Note to 'The Factory' Planning Application: Factory Façade Changes 2020 (Ref. 119890/VO/2018)

Introduction

The Proposed Development, henceforth referred to as 'The Factory' is intended to be a flexible arts and events space which forms part of the St. John's Masterplan area in Manchester City Centre. The site was acquired in March 2014 by Manchester Quays Limited (MQL), a joint venture between Allied London and Manchester City Council, and is the subject of long term regeneration proposals that involve both conversion of existing buildings and redevelopment of other plots to create a new neighbourhood of the City.

As you will be aware, Planning Permission was originally granted for the scheme in January 2017, alongside Listed Building Consent for works to the grade-II listed Colonnaded Viaduct approved in January 2017 (amendment of application ref no 114294/VO/2016). Following some further design development work during 2017/18, a revised application for Planning Permission was subsequently submitted and approved in July 2018 (Planning Ref: 119890/VO/2018).

It is understood that there is now a requirement under current fire safety regulations to amend the proposed cladding to 'the Factory' site. The implications of this on the surrounding Historic Environment will be evaluated below in line with NPPF (2019) policy and Historic England Guidance (Good Practice Advice Note 2: Managing Significance in Decision-Taking in the Historic Environment; Good Practice Advice Note 3: The Setting of Heritage Assets, as amended in December 2017 and Advice Note 12: Statements of Heritage Significance, October 2019).

Background Information

In October 2016, Heritage Architecture Ltd. submitted a Heritage Statement and ES Chapter to evaluate the potential impact of the proposals on the surrounding historic environment. Including, 18 listed buildings, 2 conservation areas and 1 'non-designated heritage asset' respectively. The non-designated heritage asset identified previously is situated directly adjacent to the Factory Site and is widely referred to as 'The Bonded Warehouse'.

SLHA identified the building as being of some architectural and historic interest in the initial Heritage Statement (12.10.2016, page 35) which informed and was appended to the Heritage Chapter of the Environmental Statement. The heritage value of the (then unlisted) **Former Grape Street Bonded Warehouse** was identified as being of some significance in Chapter 8 of the ES. It was consequently evaluated in the same terms with which we assessed the designated listed buildings, with particular reference to its sensitivity as a heritage asset within the Castlefield Conservation Area to the potential impact of the Factory development.

Since the original application was submitted, however, the **Bonded Warehouse** was **listed at grade-II** as the 'Former Grape Street Bonded Warehouse' (31st July 2018). Please refer to Appendix I for the full list description.

The purpose of the addendum note, therefore is address the potential impact of the proposed changes to the façade of the Factory Site upon the historic environment and consider the impact to the Bonded Warehouse in line with its new designated status. The overarching aim being to determine whether these developments will result in any change to the conclusions made in the Heritage Statement (SLHA, 12.10.2016) and subsequent ES Chapter.

Brief Significance Appraisal of the Bonded Warehouse

The Grape Street Bonded Warehouse (originally known as the Charles Street Warehouse pre 1891) was constructed in 1869, with a railway line passing over Water Street directly into the building for securely storing goods such as alcohol and tobacco. The brick arched side of the viaduct corresponds to this original railway line.

The Warehouse had three internal track runs from north to south and a principal internal railway line which ran along its southern side; this had a further parallel line outside the building from which sections of railway track branched into the warehouse's three large goods entrances.

Today, Grape Street runs through the middle of the former ITV Granada Studios site and is no longer a public highway. It was still used by the studios until 2013 for access and still appears in a street format. The street was bought by Granada after the neighbouring Liverpool Road Goods Station had been bought by the City Council in the late 1960s. Granada's acquisition of the street also included the purchase of the Bonded Warehouse and goods yard to the rear of the Liverpool Road Goods Station.

The Bonded Warehouse, and much of the former ITV estate was acquired by Allied London's in 2014, proposals to redevelop the Bonded Warehouse as part of the St. Johns Masterplan began to emerge (c.2015). The grade-II listed building has since been redeveloped, offering co-working spaces and studios.



Figure 1: The former Bonded Warehouse, north elevation, as seen in an oblique aerial view from 2015. (SLHA)

Bonded Warehouse: Contribution of setting to significance

The Warehouse is understood and appreciated as an integral part of the development of the railways in this part of the city. The surrounding railway buildings, many of which are listed, make a positive contribution to how the Bonded Warehouse is understood and appreciated within its wider setting. Low quality structures, associated with the development of the former ITV Quay Street estate, were constructed during the late-20th century adjacent to the Warehouse. It is therefore considered that there is high capacity for change and even the potential to enhance the setting of the Bonded Warehouse. Note, the Factory development is currently under construction to the east.

Overview of Façade changes and Visual Impact Assessment

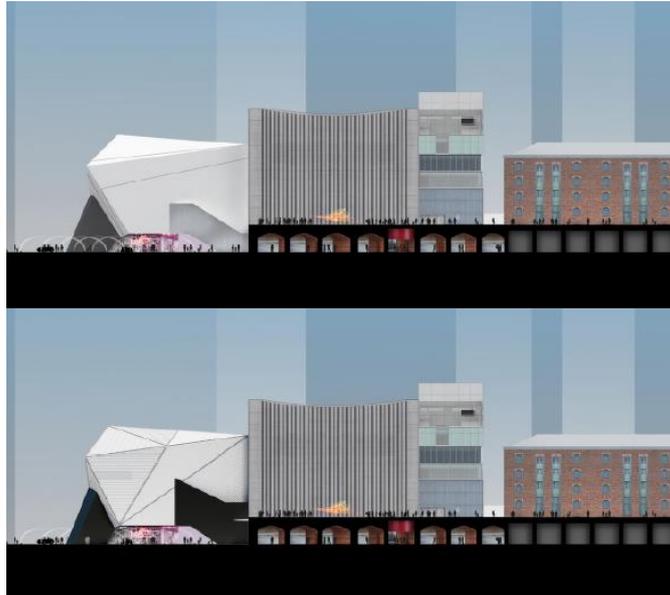


Figure 2: The consented 'Factory' is indicated on the CGI adjacent to the Bonded Warehouse. The revised cladding is depicted in the bottom image.

Figure 2 (above) illustrates the consented Factory development. The Bonded Warehouse is illustrated directly to the east of the Proposed Development. The top image illustrates the consented development / cladding, this has now been revised to comprise corrugated metal cladding to the west and a translucent curtain wall to the east. The image below provides an indication of the negligible visual change which will result as part of the revised façade.

Figure 3 below illustrates the north elevation of the grade-II bonded warehouse looking west towards the consented Factory development (currently under construction). The indicative views of the consented Factory Development (Figure 3: 2018 above, with revised 2020 proposals below) indicate the proposed change to the consented development to introduce a translucent curtain wall will result in negligible visual change.



Figure 3: illustrates the proposed development from a key viewpoint, looking west with the north elevation of the Bonded Warehouse in full view. The top image depicts the 2018 proposals alongside the revised (below).

Conclusions

The images above demonstrate that the minor changes to the façade treatment will result in no change in perceived heritage impact upon the now grade-II listed Bonded Warehouse or any other designated heritage assets within the wider townscape.

It will be clear that the change in status of the building does not change the assessment of heritage impact, as the 2016 heritage assessment specifically considered the impact on the building's 'setting'. The architectural and historic interest of the listed building is most clearly appreciated (or experienced) at the immediate street-level, along Grape Street and Water Street (from where architectural details such as the distinct polychromatic brick can be viewed). The special interest of the now grade-II listed building would not be compromised by the proposed façade changes to the consented Factory development.

The listing and change in designated status formalise the building as a heritage asset but does not alter how the building should be evaluated in terms of its juxtaposition to the proposed development. Consequently, the impact of the approved development was fully considered at the ES 2017 stage.

Thus, we remain of the view that the approved development would not diminish the appreciation or experience of the former Grape Street Bonded Warehouse as a heritage asset and that the conclusions outlined in both the Heritage Statement and ES 2018 remain valid.

Sincerely,

Sarah Stirling
BA (Hons), MA
Heritage Architecture Ltd.

Appendix I: List Description

Location Description: Quay Street, Manchester, M3 3JE.

District: Manchester (Metropolitan Authority)

Parish: Non Civil Parish

National Grid Reference: SJ8307297946

Summary

Former railway bonded warehouse, built 1867 to 1868 for the London and North Western Railway; it underwent internal modifications in the C20 and early C21.

Reasons for Designation

The former Grape Street bonded warehouse, Quay Street, Manchester, constructed between 1867 and 1868, is listed at Grade II for the following principal reasons:

*Architectural interest: * it has a sophisticated multi-level design, and an impressive, well-detailed external appearance that includes good polychromatic brick work and ashlar decoration; * it is relatively little altered externally, and retains distinctive features relating to its original use including substantial ground-floor goods doors, and a regular arrangement of windows and taking-in doors; * despite the late-C20 and early-C21 internal subdivisions and modifications, it is understood that the principal original internal structure survives well, including cast-iron columns, jack-arch and timber floors, and a queen-post roof with the remains of hoists.*

*Historic interest: * it forms part of the legacy of C19 large-scale railway warehouses in Manchester, and is a good illustration of the transport heritage of one of the country's most important industrial cities.*

*Group value: * it is part of an important group of listed railway structures including Liverpool Road Station (Grade I), the world's first large-scale railway warehouse (Grade I), two further warehouses and stores to the south-east (both Grade II) and various listed railway bridges and viaducts.*

History

The former Grape Street bonded warehouse was constructed between 1867 and 1868 between Liverpool Road Station and Grape Street (originally known as Charles Street), as part of a complex of goods warehousing associated with the former Liverpool Road Station (listed Grade I). The station was built in 1830 (the oldest surviving terminal railway passenger station in the world) along with a warehouse opposite (the world's first large urban railway warehouse; listed Grade I). In 1844, following the opening of Manchester's Victoria Station, Liverpool Road Station became a goods depot under the ownership of London and North Western Railway. By 1851 two cotton stores had been built to the north-east of the station and in around 1855 a goods shed was built on the corner of Liverpool Road and Lower Byrom Street (listed Grade II). In 1866 a fire destroyed the cotton stores, which were replaced by the former Grape Street bonded railway goods warehouse. A further warehouse was built on Lower Byrom Street in 1888 (listed Grade II).

The former Grape Street bonded warehouse was a secure building where goods were stored in advance of any duty payments being made. The building is marked on historical late-C19 and C20 Ordnance Survey Maps as both a goods shed and warehouse and it is unclear whether it was used as a general goods store before it officially became a bonded warehouse. Originally goods were brought in by rail tracks, carried on a brick-vaulted viaduct which ran in front of the warehouse's south elevation; some of the viaduct arches in front of the warehouse were used as bonded stores. There were three turntables in front of the large doors on the south elevation that directed the tracks into the building. Three further turntables were located within the warehouse, linked to an internal railway track network. This included a track which ran through openings in the east and west elevations and along the south end. The ground floor on the north side of the building was at a lower

street level. In the late C19 a timber yard was established on the east side of the warehouse, and between 1890 and 1908 was replaced by two parallel rectangular buildings containing stables, stalls and a harnessing room. The warehouse remained in operation as part of the wider railway goods complex until 1975, after which the building was sold.

In the mid-C20 Granada Television Centre was established on the north side of Grape Street and by the 1980s it had expanded south to incorporate part of the former goods depot. The bonded warehouse was retained and converted to form part of the television studios which included the insertion of lifts, staircases, WC facilities and associated plant. The adjoining stables were also retained and converted into an experimental theatre with a performance space and a bar. New buildings were added to the site including those constructed above adjacent viaduct. The exterior television set for the soap opera *Coronation Street* was also built to the east side of the stables in 1982. In 1989 the basement and ground-floor levels of the bonded warehouse were further remodelled internally when the building was converted for use as part of the Granada Tours Scheme (the tours ended in 1999). Part of the single-storey stable range was converted to create a copy of the *Coronation Street's Rover's Return*, including a replica facade, to provide tour customers with a place to drink. Granada left the site in 2007. Since then parts of the warehouse have been reused on a temporary basis. The building is currently (2018) subject to internal and external refurbishment.

Details

Former railway bonded warehouse, built 1867 to 1868 for the London and North Western Railway; it underwent internal modifications in the C20 and early C21.

MATERIALS: red-brick construction with blue-engineering-brick detailing including banding and quoins, with Welsh-slate roof coverings.

PLAN: the warehouse has a rectangular footprint and is orientated east-west. There are six floors including a lower and upper ground floor.

EXTERIOR: five-and-a-half storeys, with four storeys to the south side where the ground level is higher, beneath hipped roofs of slate. All windows have segmental heads, brick voussoirs, blue-brick keystones and reveals, ashlar springers and cills, and are fitted with metal multi-pane casements. Taking-in doors are timber with glazed uppers. Ground floor entrances are fitted with double battened timber doors.

The 12-bay north elevation has six loading bays arranged in three pairs, with taking-in doors to the first to fourth floors, and the remains of brackets for the hoists above. The ground floors of the loading bays have small window openings. The other bays have tall, wide, arched entrances to the ground floor, and arched windows to the upper floors. The six-bay west elevation has two loading bays with taking-in doors and three lower ground-floor doors. The ground level at the southern end of this elevation is higher and there is another door at first-floor level (formerly access for an internal railway line). The 12-bay south elevation has three large first-floor doors where the railway tracks would have originally entered the warehouse. The remainder of this elevation has a similar arrangement to the north elevation, but has three single loading bays. The east elevation is largely blind, with a regular arrangement of arched recesses and there is a first-floor door to the higher south end (similar to that on the west elevation). Part of the east elevation's lower floors is obscured by the attached former stable block. The building has a dentilled parapet. At the top of the west elevation is a painted sign reading BONDED WAREHOUSE. Behind the parapet is the twin-hipped roof housing some C20 plant.

INTERIOR: it was not possible to gain access to the buildings, and the following description is based on other available information. All of the floors are supported by cast-iron cruciform and circular-hollow columns. The lower and upper ground-floor levels are also supported by substantial brick piers, some of which have blue-brick quoins and chamfer detailing to the corners. Along the southern edge of the lower ground-floor are sections of brick arches. The three lower floors are fire-proof constructions with wrought-iron girders and brick-jack arch ceilings. The upper three levels have timber floors. Most of the ceilings are covered in modern ceiling tiles. The lower four floors are subdivided by C20 and C21 plasterboard and concrete-block partitions to form office and studio spaces, and there are several C20 and C21 inserted stairs and lift shafts, particularly on the north side of the building. The upper two floors remain as large open spaces. The top floor is open to the roof and is topped by a twin queen-post roof structure, within which there are the remains of the original hoist machinery.