

Kinnell Mill, Kinnell, Friockheim, Angus:  
Standing Building Recording and Metal Detecting Survey

Data Structure Report



by Peter Klemen  
issued 19<sup>th</sup> February 2020  
On behalf of Mr Guthrie Batchelor



## Quality Assurance

This report covers works which have been undertaken in keeping with the issued brief as modified by the agreed programme of works. The report has been prepared in keeping with the guidance of AKD Archaeology on the preparation of reports. All works reported on within this document have been undertaken in keeping with the Chartered Institute for Archaeologists' Standards and Policy Statements and Code of Conduct.

Signed

Date .....19<sup>th</sup> February 2020....

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## Quality Assurance Data

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## Introduction and Planning Context

1. This Data Structure Report represents the findings of a two phase programme of archaeological works; Historic Building Recording and Metal Detecting Survey that was required by Kerry Smith Architects on behalf of Mr Guthrie Batchelor in advance of the demolition of a current WWII structure and then erection of a dwellinghouse and garage at Kinnell Mill, Kinnell at Kinnell Mill, Kinnell, Friockheim, Angus, DD11 4UL (NGR: NO 60930 50613).
2. The development has been granted planning consent by Angus Council (Planning Ref: Ref: Ref: 18/00988/PPPL) subject to a negative suspensive condition for archaeology (PAN 2/2011, SPP, HES Policy Statement 2016). Aberdeenshire Council Archaeology Service, who advise Angus Council on archaeological matters, has provided guidance on the structure of Historic Building Recording works required on this site to inform the planning process.
3. In particular they have specified a Level 1 survey in accordance with Recording Historic Buildings, A Descriptive Specification (RCHME Third Edition, 1996), an Appraisal survey in accordance with Historic Building Recording Guidance (ALGAO: Scotland, 2012) and a linked Metal Detecting Survey across the proposed development area.
4. The proposed development is located to the south of the former World War II Kinnell Airfield/ RAF Kinnell (Canmore ID: 94182) and covered within the Historic Environment Record area (ACAS: HER NO65SW0049). The standing structural elements are those associated with the former RAF Kinnell.
5. AKD Archaeology was appointed by Mr Guthrie Batchelor to act with regard to the archaeological issue. The project works were designed to comply with the identified requirements of Aberdeenshire Council Archaeology Service in advance of determination.

## Historical Background

6. The purpose of the building recording is to investigate and record the surviving elements of the structure, with the purpose to provide a baseline record of the extant structure prior to the demolition. Annotated plans of the current site layout (Figures 1 & 2) were compiled and provided Kerry Smith Architects (Figures 1 & 2) and Mr Guthrie Batchelor for use by AKD Archaeology.
7. The proposed development requires the current surviving structure associated with Kinnell Airfield/ RAF Kinnell to be demolished. The area in which Kinnell Airfield is located was prior to 1942 an open area of fields (Figure 3a) with the current area farmland. With reference to the proposed development it is located on the south side of the former airfield and most likely functioned as a storage building. The two runways formed a 'V-shape'; one aligned southeast-northwest the other roughly north-south, with a perimeter track enclosing the overall area of the airfield (Figures 3b & 4a). The current structure is located just outwith the perimeter track. The runways at Kinnell Airfield differ from the typical layout of wartime runways where they had three runways and laid out to form an 'A-shape' (Historic England 2019). A comparative runway layout is noted at Balado Bridge, Perth & Kinross.
8. After the war had started it was quickly recognised that many more airfields would be required. These new airfields constructed during wartime were built to temporary standards and required less expense and manpower, and as such they were not expected to last beyond the end of the war (Historic England 2019). Due to the increased threats from bombing the wartime airfields, bases and associated structures were scattered to minimise these affects. In contrast the pre-war airfields, bases and associated structures were close and compact units (Historic England 2019).
9. Opened in March 1942 Kinnell Airfield/RAF Kinnell spent its brief life firstly as a satellite to the airfield known as RAF Tealing (NO43NW0051) and after 1944 as a satellite airfield to Montrose (NO75NW0031). It's use mainly by No 56 Operational Training Units (OTU) flying Hawker Hurricanes and Miles Masters. By the time the unit disbanded in the summer of 1944, Supermarine Spitfires accompanied the Hawker Hurricanes at the airfield (Airfields of Britain Conservation Trust 2019).



10. Kinnell then became home to the Miles Masters and Airspeed Oxfords of No 2 Flying Instructors School from Montrose. Kinnell was the unit's primary satellite between September 1944 and July 1945. After the final Flying Instructors School disbanded in 1945 the site was used for aircraft storage by No 44 Maintenance Unit (MU), based at Edzell, until around 1946/47, as well as equipment storage by No 260 MU, based at Errol, into 1948 (Airfields of Britain Conservation Trust 2019).
11. The first aircraft to be based at the airfield were Westland Lysanders from the target-towing Flight of the OTU, which was used to train fighter pilots. Kinnell remained a satellite for the unit, which was renamed as No 1 Combat Training Wing and later No 1 Tactical Exercise Unit. The role primarily continued to be fighter training, night flying and with some fighter-bomber instruction also taking place.
12. The significance of the OTU's is that prior to any pilots or crew joining an operational squadron, they would spend time at an OTU learning together to work as a crew in the case of the bombers crews and learning more in depth training for the individual pilots (Historic England 2016). The training provided at an OTU was essentially a "postgraduate course" (Historic England 10: 2016) in flying and technical skills.
13. The Angus HER (ACAS: NO65SW0049) compiled by Herbert. C (08/05/2019) provides a detailed outline of the layout and elements of RAF Kinnell. The southern runway is cut by a minor road leading north from Kinnell to Pitmikie (Figure 4a). There were two dispersal areas, to the north-east and south of the runways, with three accommodation camps to the north-west.
14. The accommodation camps were laid out in three areas which lie to the north-east of the airfield. The first area, a small group of Nissen huts set out in an L-shape was situated at; NO 6160 5230, about 300m west of the farmstead and had been removed by 1953. The second area, within the same field at; NO 6170 5198, is a larger group of huts within a square enclosure, probably partially removed by 1953 and not shown on the current OS map. The final group at; NO 61374 52266, was the largest. A combined three hut building and a detached hut to the west survive in use from this group.
15. Little now remains of the structural elements of the airfield, but the layout of the airfield and its camps are recorded by vertical aerial photographs taken by the Luftwaffe in 1943 and by the RAF in 1946 and 1953. These images show that the northern dispersal area had two blister type aircraft hangars, at least four buildings or large huts and fifty huts with three emergency air-raid shelters. At least two of the larger huts are still shown as roofed on the 2006 map.
16. The sites of all the hangars and their access routes are still visible, but the hanger buildings have been removed. The southern dispersal area had one blister type aircraft hanger, one large and several smaller huts, at least one of which is shown on the 2006 map. The remaining elements of the surviving structure are most likely associated with this aircraft hanger. The remaining blister hanger and its associated buildings were to the north-west of the control tower and battle headquarters (themselves on the west of the runways) and some of these buildings are shown on the current OS map.
17. The battle headquarters is visible on the RAF vertical photographs as a small square flat roofed building, it now is visible as a small square, ruinous, building within a low oval mound. There would have been an underground chamber, which may still exist below the square building. There are two control buildings, both of which survive in ruinous condition, the original, a satellite fighter watch office, survives as a single storey building immediately to the southeast of the later two-storey tower.

## Project Works

18. The programme of building recording and metal detecting was undertaken over one site visit which took place on the 17<sup>th</sup> February 2020 and the agreed programme of mitigation works were to comprise the following key components:

- a programme of historic building recording carried out on the surviving elements of the WWII structure. The historic building recording was undertaken to an Appraisal Level (Level 1) survey in accordance with Historic Building Recording Guidance (ALGAO: Scotland, 2012).
  - a metal detecting survey over the area of the proposed development with the aim to retrieve artefacts with its former use.
  - the production of a report which integrates the findings of the works (HBR) through a Data Structure Report, detailing the nature, form and extent of the historic building(s) and any artefacts.
19. All work was undertaken in accordance with the Written Scheme of Investigation (Klemen 2020) previously agreed with Aberdeenshire Council Archaeology Service and complied with the Chartered Institute of Field Archaeology's Standards and Policy Statements and Code of Conduct.
  20. The proposed development is located to the south of the former World War II Kinnell Airfield (Canmore ID: 94182) and covered within the Historic Environment Record area (ACAS: HER NO65SW0049). The standing structural elements are those associated with the former airfield. The proposed development requires the present structural elements to be demolished (Figures 2, 4b, 5a & 5b).

## Findings: Building Recording & Metal Detecting Survey

### *Structural elements*

21. The purpose of the building recording was to investigate and record the surviving elements of the WWII building which made up an element of the former RAF Kinnell. The aim of the exercise was to provide a baseline record of the extant structure prior to the demolition as part of the proposed development for a residential dwelling. Annotated plans of the current site layout (Figures 1 & 2) were provided by the client for use by AKD Archaeology and were produced by Kerry Smith Architects.
22. Overall the building is rectangular in plan and aligned north-south measuring 9m (E-W) by approximately 17m (N-S) (Figures 1 & 2). The only standing element of the building is located in the northwest corner of the building and rectangular in plan with dimensions 3m (N-S) by 4.2m (E-W) (Figure 2). The standing element of the former building is constructed of red brick, single skinned with an approximate maximum height of 4.3m (Figures 4b, 5a & 5b).
23. There is very little detail on the north and east elevations (Figures 2, 6a & 6b) of the extant structure with grey concrete render clearly visible and suggestive that the exterior of the building would have been covered in the same render. The east elevation represents what would have been the interior of the building when standing and demonstrates the red brick construction material.
24. The south elevation (Figures 2, 7a and 7b) again demonstrates the red brick construction material and has one window and doorway. The window measures 1.85m wide by 1.30m in height with a cast concrete lintel measuring 2.30m in length by 180mm thick. The doorway measures 2m high by 870mm wide and again with a cast concrete lintel measuring 1.20m in length and 180mm thick.
25. As with the north elevation the west elevation is covered by concrete render and further supports the suggestion that the whole exterior was covered in concrete render, there is also one window (Figures 2, 8a & 8b). The dimensions measure 1.04m wide and 1.65m in height, although there is brickwork missing at the top of the window outline and therefore the height is only recorded from the present conditions. During clearing areas of vegetation for the metal detecting survey the original steel framed window from the west elevation was recovered (Figure 9a).
26. On closer inspection it was possible to observe the buildings southern end (Figure 9b). Although covered in vegetation the southwest corner was visible along with the whole width of the wall standing to a height of 700mm.



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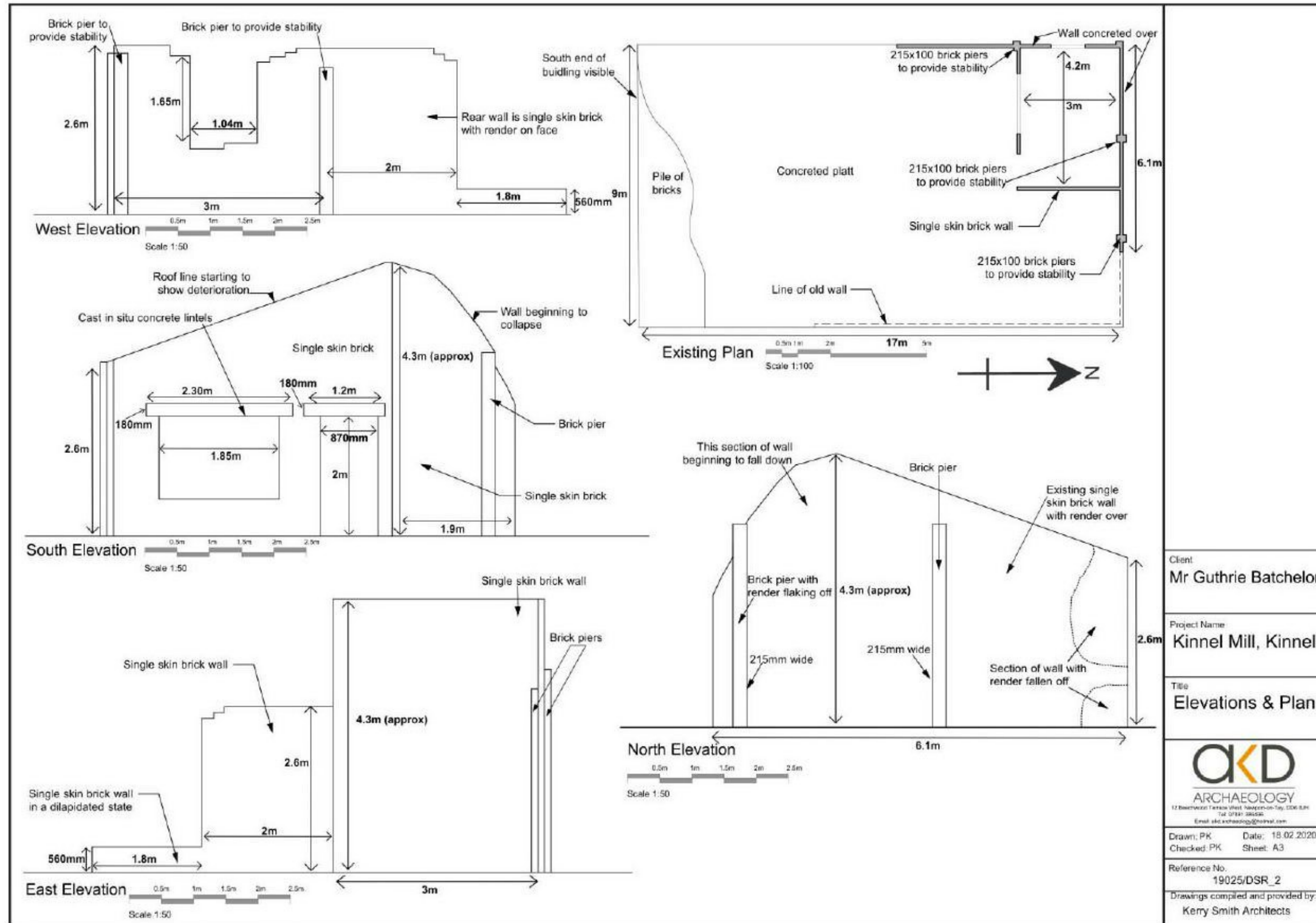


Figure 2: Plan and elevations.





Figure 3a: Extract from the 6-inch 3<sup>rd</sup> edition Ordnance, Forfarshire Sheet XL.NW (includes: Guthrie; Inverkeilor; Kinnell; Kirkden) (1926). Green oval denotes site location.

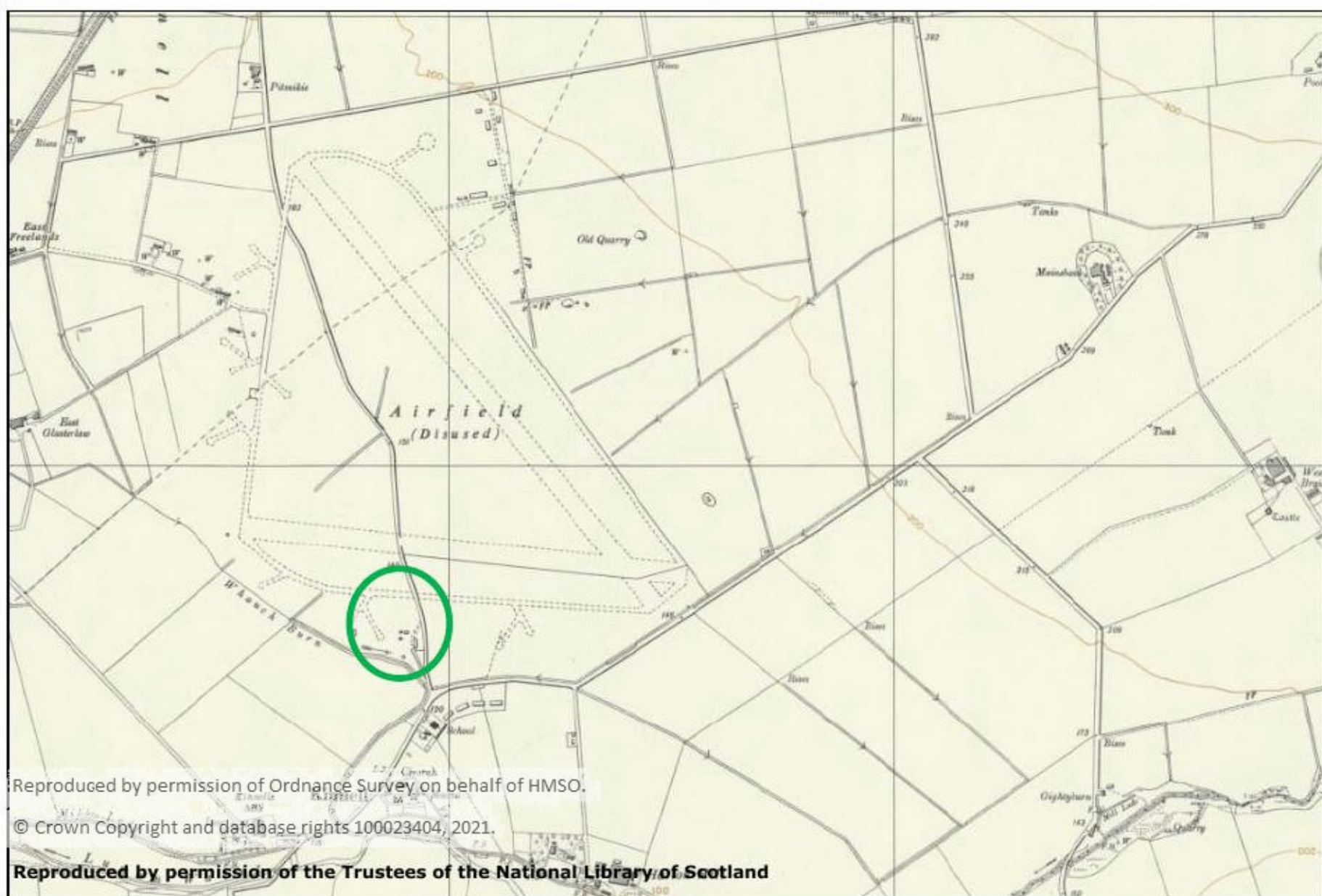


Figure 3b: Extract from the National Grid Maps NO65SW - A 6-inches to 1 mile (includes: Craig; Farnell; Inverkeilor; Kinnell) (1959). Showing the area of the former Airfield. Green circle denotes site location.



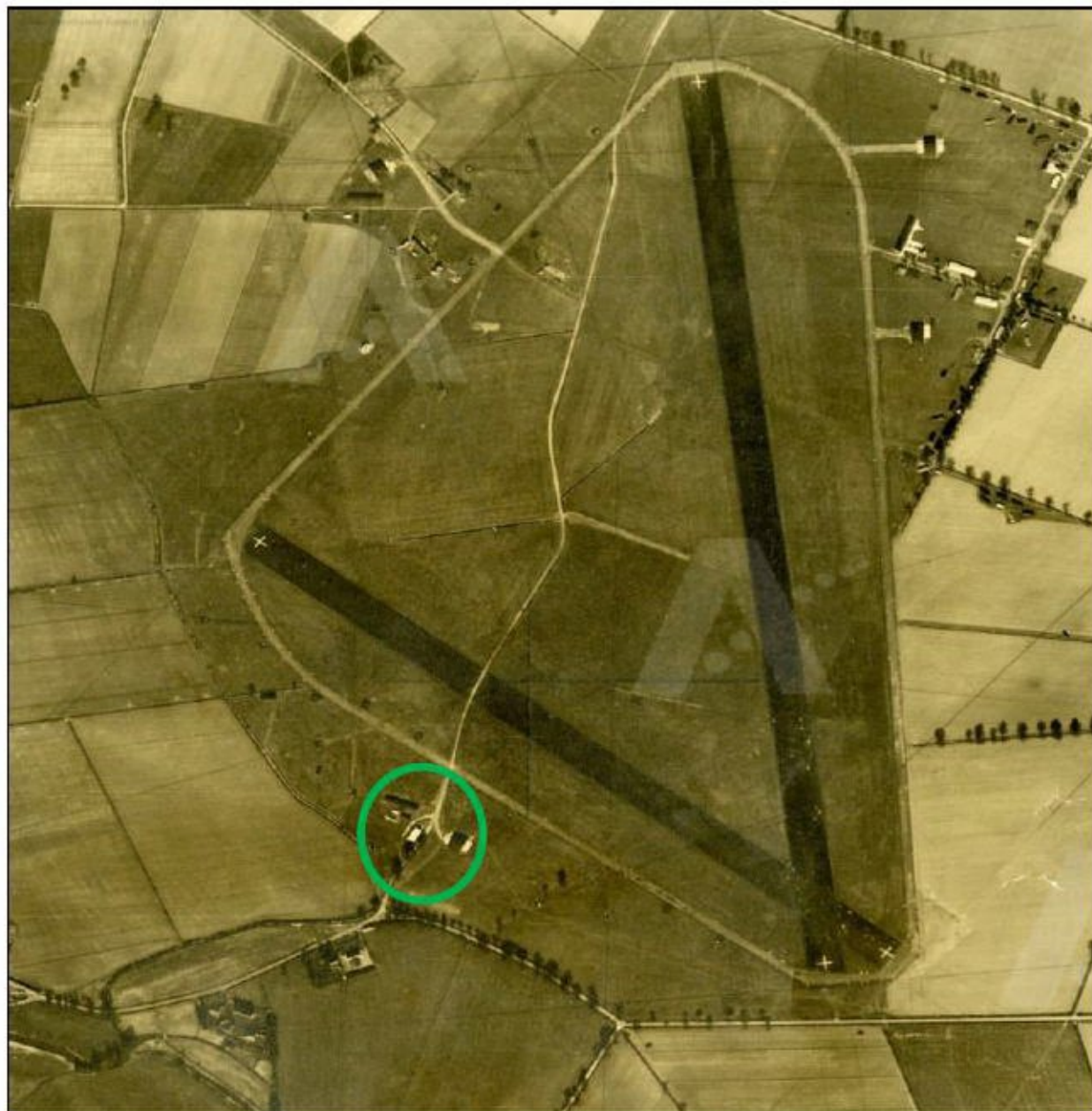


Figure 4a: Aerial photograph showing Kinnell Airfield and the site location denoted in green with a white roof representing the structure. From Aberdeenshire Council Archaeology Service: Angus HER (<https://online.aberdeenshire.gov.uk/smrpub/master/detail.aspx?refno=NO65SW0049>, 10/02/2020).



Figure 4b: North elevation of the surviving structural elements.





Figure 5a: South elevation of the surviving structural elements.



Figure 5b: South elevation of the surviving structural elements and original concrete track (left of image).





Figure 6a: North Elevation.



Figure 6b: East Elevation.





Figure 7a: South elevation.



Figure 7b: Interior: North elevation.





Figure 8a: Interior: West elevation.



Figure 8b: Interior: East elevation.



### *Metal Detecting Survey*

27. All accessible areas of the proposed development area were included in the metal detecting survey (Figure 1). The instrument used was a *White's Coinmaster* metal detector with a round 9-inch spider/concentric coil. In some areas the thick vegetation made 'sweeping' the metal detector difficult but were still able to be surveyed.
28. There was a great deal of interference caused by large amounts of metal in the form of modern fencing (Figure 10a) that had fallen and also modern detritus in the form of items such as aluminium cans (Figure 10b) that had become incorporated into the upper levels of the topsoil and just under the vegetation cover. The use of metal within the construction of the structure in the form of reinforcement bars and rivets for roofing also produced a great deal of background noise.
29. Unfortunately the metal detecting survey failed to recover any artefacts associated with the structure's former use associated with RAF Kinnell. Where the metal detecting survey recorded roofing rivets these were still connected to corrugated asbestos sheeting. This is suggested to have been the original roofing material.

### Discussion

30. The surviving standing elements of the former WWII structure most likely formed a small office space within the larger rectangular building. The presence of the two windows; one on the west elevation providing light from the outside and one on the south elevation that would have looked onto the interior of the building. The doorway, also on the south elevation, enabled access to and from the interior of the building.
31. From the aerial photograph (Figure 4a) the southern dispersal area is visible with one blister type aircraft hangar, one large and several smaller huts. The original concrete dispersal area is still visible (Figure 11a). The larger of the huts has a distinct white roof and the remaining elements of the surviving structure are most likely associated with this structure.

### Recommendations

32. This Data Structure Report represents the findings of a Historic Building Recording and the Metal Detecting Survey required by Mr Guthrie Batchelor. The historic building recording survey confirmed that there was very little the original Word War Two structure still surviving.
33. The linked metal detecting survey also failed to recover any artefacts associated with the structure's former use for storage associated with RAF Kinnell. This is primarily due to the presence of a lot of metal in the form of modern fencing that had fallen and become incorporated into the upper levels of the topsoil and also within the covering vegetation. The use of metal within the construction of the structure in the form of reinforcement bars and rivets for roofing also produced a great deal of background noise.
34. As such it is the recommendation of AKD Archaeology Ltd that no further works are appropriate. The appropriateness and acceptability of our recommendations rest with Aberdeenshire Council Archaeology Service on behalf of Angus Council.

### Conclusion

35. A two phase programme of archaeological works; Historic Building Recording and Metal Detecting Survey that was required by Kerry Smith Architects on behalf of Mr Guthrie Batchelor in advance of the demolition of the current WWII structural elements and then erection of a dwellinghouse and garage at Kinnell Mill, Kinnell at Kinnell Mill, Kinnell, Friockheim, Angus, DD11 4UL (NGR: NO 60930 50613).





Figure 9a: Original steel framed west elevation window found covered by vegetation.



Figure 9b: Partially standing remains of the building's southern end.





Figure 10a: Modern metal fencing recorded during the metal detecting survey.



Figure 10b: Modern aluminium can recorded during the metal detecting survey.





Figure 11a: Surviving concrete for the southern dispersal area and Blister Hanger.

36. This Data Structure Report represents the findings of a Historic Building Recording and the Metal Detecting Survey required by Mr Guthrie Batchelor. The historic building recording survey confirmed that there was very little the original Word War Two structure still surviving.
37. The linked metal detecting survey also failed to recover any artefacts associated with the structure's former use for storage associated with RAF Kinnell. This is primarily due to the presence of a lot of metal in the form of modern fencing that had fallen and become incorporated into the upper levels of the topsoil and also within the covering vegetation. The use of metal within the construction of the structure in the form of reinforcement bars and rivets for roofing also produced a great deal of background noise.
38. As such it is the recommendation of AKD Archaeology Ltd that no further works are appropriate. The appropriateness and acceptability of our recommendations rest with Aberdeenshire Council Archaeology Service on behalf of Angus Council.



## References

### *Bibliographic*

Airfields of Britain Conservation Trust. 2019. Viewed 10/20/20.  
<http://www.abct.org.uk/airfields/airfield-finder/balado-bridge-kinross/>

Historic England. 2016. Nine Thousand Miles of Concrete: A review of Second World War temporary airfields in England. Historic England.

Historic England. 2019. Historic Military Airfields.  
<https://historicengland.org.uk/research/current/discover-and-understand/military/historic-military-airfields/>

Klemen, P. 2020. *Kinnell Mill, Kinnell, Friockheim, Angus: Standing Building Recording and Metal Detecting Survey Written Scheme of Investigation*. Unpublished report.

### *Cartographic*

Ordnance Survey	1926	6-inch 3 <sup>rd</sup> edition Ordnance, Forfarshire Sheet XL.NW (includes: Guthrie; Inverkeilor; Kinnell; Kirkden)
Ordnance Survey	1959	National Grid Maps NO65SW - A 6-inches to 1 mile (includes: Craig; Farnell; Inverkeilor; Kinnell)



## Appendix 1: Discovery &amp; Excavation in Scotland

<b>LOCAL AUTHORITY:</b>	Angus
<b>PROJECT TITLE/SITE NAME:</b>	Kinnell Mill, Kinnell, Friockheim, Angus
<b>PROJECT CODE:</b>	19025
<b>PARISH:</b>	Kinnell
<b>NAME OF CONTRIBUTOR:</b>	Peter Klemen
<b>NAME OF ORGANISATION:</b>	AKD Archaeology
<b>TYPE(S) OF PROJECT:</b>	Historic Building Recording & Metal Detecting Survey
<b>NMRS NO(S):</b>	None
<b>SITE/MONUMENT TYPE(S):</b>	None
<b>SIGNIFICANT FINDS:</b>	None
<b>NGR</b> (2 letters, 6 figures)	NO 60930 50613
<b>START DATE</b> (this season)	17 <sup>th</sup> February 2020
<b>END DATE</b> (this season)	17 <sup>th</sup> February 2020
<b>PREVIOUS WORK</b> (incl. DES ref.)	None
<b>MAIN (NARRATIVE) DESCRIPTION:</b> (may include information from other fields)	<p>A two phase programme of archaeological works; Historic Building Recording (Level 1) and Metal Detecting Survey was required by Mr Guthrie Batchelor in advance of the demolition of the current WWII structure and then erection of a dwellinghouse and garage at Kinnell Mill, Kinnell at Kinnell Mill, Kinnell, Friockheim, Angus, DD11 4UL (NGR: NO 60930 50613).</p> <p>The development was granted planning consent by Angus Council (Ref: 18/00988/PPPL) subject to a negative suspensive condition for archaeology (PAN 2/2011, SPP, HES Policy Statement 2016). The works were designed to create a competent record of the historic elements of the WWII structure before loss. The aim of the metal detecting survey is to retrieve any artefacts associated with its use.</p> <p>Nothing was recorded during the metal detecting survey. As such AKD Archaeology recommends that no further archaeological works are required relating to planning application (Ref: 18/00988/PPPL).</p>
<b>PROPOSED FUTURE WORK:</b>	None
<b>CAPTION(S) FOR ILLUSTRS:</b>	None
<b>SPONSOR OR FUNDING BODY:</b>	Mt Guthrie Batchelor
<b>ADDRESS OF MAIN CONTRIBUTOR:</b>	12 Beechwood Terrace West, Newport-on-Tay, Fife, DD6 8JH
<b>E MAIL:</b>	akd.archaeology@hotmail.com
<b>ARCHIVE LOCATION</b> (intended/deposited)	Report to Aberdeenshire Council Archaeology Service and archive to National Record of Historic Environment (NRHE).



## Appendix 2: Photo Register

### *Photographic Register:*

Image No.	Digital	Description	From	Date
01	8554	Setting images of building; North elevation	SW	17/02/20
02	8555	Setting images of building; North and East elevation	SSW	17/02/20
03	8556	Setting images of building; South and East elevation	NW	17/02/20
04	8557	Setting images of building	NW	17/02/20
05	8558	Setting images of building; South elevation	N	17/02/20
06	8559	Setting images of building; South elevation	N	17/02/20
07	8560	Setting images of building; South elevation	NE	17/02/20
08	8561	Setting images of building; South elevation	NE	17/02/20
09	8562	Setting images of building; including original concrete track	N	17/02/20
10	8563	Setting images of building; North and West elevation	E	17/02/20
11	8564	Area of former dispersal hanger and concrete standing	E	17/02/20
12	8565	Exterior; East elevation	W	17/02/20
13	8566	Exterior; South and East elevation	NW	17/02/20
14	8567	Exterior; South elevation (south side)	N	17/02/20
15	8568	Exterior; South elevation	N	17/02/20
16	8569	Exterior; South elevation	N	17/02/20



Image No.	Digital	Description	From	Date
17	8570	Exterior; East elevation (south side)	W	17/02/20
18	8571	Exterior; whole of east elevation	W	17/02/20
19	8572	Interior; South elevation	N	17/02/20
20	8573	Interior; South elevation	N	17/02/20
21	8574	Interior; West elevation	E	17/02/20
22	8575	Interior; upper half of West elevation	E	17/02/20
23	8576	Interior; upper half of North elevation	S	17/02/20
24	8577	Interior; doorway on North elevation	S	17/02/20
25	8578	Interior; North elevation	S	17/02/20
26	8579	Interior; North elevation (with flash)	S	17/02/20
27	8580	Interior; doorway on North elevation (with flash)	S	17/02/20
28	8581	Interior; East elevation	W	17/02/20
29	8582	Interior; East elevation (with flash)	W	17/02/20
30	8583	Exterior; West elevation	E	17/02/20
31	8584	Exterior; West elevation	E	17/02/20
32	8585	Original steel framed window from West elevation	W	17/02/20
33	8586	Original steel framed window from West elevation	E	17/02/20
34	8587	Exterior; North elevation	S	17/02/20
35	8588	Exterior; North elevation	SE	17/02/20



Image No.	Digital	Description	From	Date
36	8589	Exterior; North elevation	SW	17/02/20
37	8590	Exterior; view onto the former interior of the building	S	17/02/20
38	8591	Exterior; view onto the former interior of the building	S	17/02/20
39	8592	Exterior; South elevation	N	17/02/20
40	8593	Exterior; South elevation showing West edge	N	17/02/20
41	8594	SW corner of the southern end of the building	NE	17/02/20
42	8595	Southern end of building; standing to a height of 700mm	N	17/02/20
43	8596	Southern end of building; standing to a height of 700mm	N	17/02/20
44	8597	Modern metal fence recovered during metal detecting survey	E	17/02/20
45	8598	Aluminium can recovered during metal detecting survey	NA	17/02/20



## Contact Details

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