



Design & Access Statement

Proposed development to extend and enhance existing Garden Centre

**Mayberry Garden Centre
Old Shoreham Road
Portslade
Brighton
BN41 1SP**

1.0 Introduction & Project Overview

This Design & Access Statement has been prepared to accompany the planning application for the extension of the Mayberry Garden Centre and the diversion of the Public Right of Way (PRoW) to the West of the Garden Centre. The site and the proposed development are described in detail below.

The Garden Centre is owned and operated by Tates of Sussex who are a well known throughout East and West Sussex, with 4 Garden Centres located in Newhaven, Hassocks and Portslade with the most recent site in Dial Post having recently opening its newly extended and refurbished café and restaurant.

The Mayberry Garden Centre is currently the smallest of the Centres and provides a wide range of popular gardening and plant products alongside the Pet & Aquatics shop and coffee shop.

This application proposes the extension of both the indoor and outdoor retail areas to provide much needed additional sales area. As part of the scheme the stock delivery process is to be addressed as currently deliveries are made in smaller vans from the Hassocks store, which increases the amount of vehicle movements to and from the site. The scheme proposes a new warehouse and 'goods in' which would be able to accommodate larger delivery vehicles, therefore simplifying the day to day operations of the Garden Centre.

To the west of the garden centre there is an area of open scrub land which has previously been used as allotments, and is still called 'Eastbrook Allotments'. The allotments site has stood vacant for several years following the release of the site in 2005 by the Secretary of State for the purpose of 'business development'. The site has subsequently been incorporated into the Adur District Council's Local Plan and the Eastbrook Allotments Development Brief (2015), and the land is consequently identified by the Council for employment generating development.



Image above: illustrative extent of Mayberry Garden Centre (n.t.s.)
Image courtesy of Google Maps - 2020

2.0 Planning History

No historic planning applications have been made on the site.

A pre-application was submitted to Adur & Worthing Councils on April 2020 for re-development of the Eastbrooks Allotments site and the extension of the Garden Centre - ref: PREAPP/0123/20.

The submitted proposals included both the extension of the Garden Centre and the re-development of Eastbrook Allotments - as indicated in the image below. The re-development of the allotments for a proposed car showroom is subject to a separate, but parallel application.

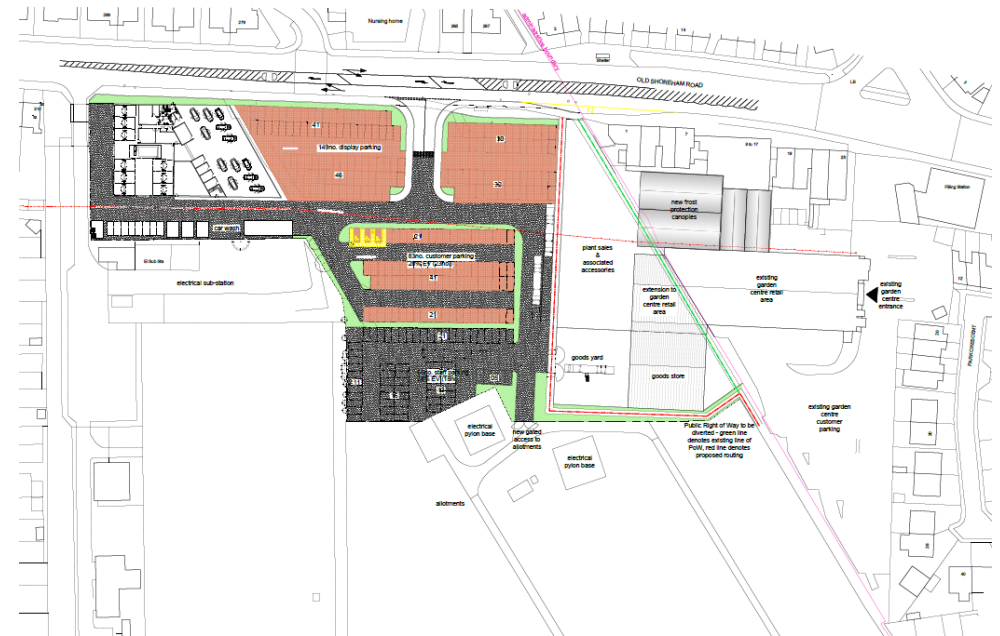


Image above: Pre-Application Proposed Site Plan (n.t.s.)

3.0 Existing Site

Mayberry Garden Centre is situated south of the A270 - Old Shoreham Road, in Portslade, with the Eastbrooks Allotments to the west of the Garden Centre.

The proposed development site envelopes the Garden Centre in its entirety and includes the eastern third of the allotment sites, including the PRoW.

The overall proposed site (redline area) is approximately 16,821m² (1.68ha) and currently comprises of a mixture of buildings, open air plants sales areas, car parking and open scrubland.

The allotments site is relatively level as a whole, with a very gentle slope north to south. The Garden Centre site again is relatively level throughout the retail areas, with the car park sloping away from the Garden Centre, with a steeply sloped area providing access to the lower ground storage area.

The site is bounded to the North by the A259 Old Shoreham Road, Mayberry Garden Centre to the East, and the remaining functioning allotments to the South. To the West lies an EDF owned electrical sub-station and the rear gardens to residential properties along Hadrian Avenue beyond.

The power cable pylons are a prominent feature on the skyline, with the cables running North to South that create a corridor of restricted development opportunities due to the inherent electromagnetic fields of the high voltage cables.

A Public Right of Way (PRoW) bisects the site, running North to South from Old Shoreham Road to Manor Hall Road and on to Fishersgate Train Station. The footpath continues North on the Northern side of Old Shoreham Road up to Mile Oak Road and beyond. The line of the footpath also forms the administrative boundary between Adur District Council to the West and Brighton & Hove City Council to the East, but falls within the control of West Sussex County Council.



Image above: illustrative extent of the proposed development site boundary (n.t.s.)

4.0 Site Constraints & Analysis

As previously mentioned above the site, like all sites, has a number of constraints which impact the proposed development of the site.

The largest constraint on the Eastbrooks Allotments site is the high voltage distribution cables and pylons which are located to the south of the site and head north away from the sub-station. The Electromagnetic Field generated by the cables limits the development potential immediately below the cables, as well as within an arc around the cables produced by the natural sag and swing of the cables. The impact of the cables is detailed on Folkes Architects drawing no. 1.05 - Electrical Constraints Plan.

UK Power Networks have previously confirmed that:
“ the Deeds of Grant states that no dwelling building or structure is to be placed within 4.6 m on either side of the electric overhead lines. It should be made that the 4.6m clearance, either side of the overhead line is after allowing for maximum sag and swing of the conductors being taken into account, and not a clearance from the conductors in still air. Allowing for 4.6 m could be anything from 9m - 15m according to the span length .”

The sites location means that there are excellent transport links to and from the site and within the immediate vicinity, with bus stops located on both sides of Old Shoreham Road, the Public Right of Way providing access to Fishersgate Train Station to the south.

The Garden Centre is a well-established business with an established vehicular access off the A259, with a fairly large customer car park with plenty of Accessible Parking spaces and a small number of cycle parking spaces.

There are also a number of buried services runs within the site as well as a 450mm dia. Southern Water mains drain bisecting the site as it runs from east to west.

Not all constraints are man-made, as the allotments site has established population of slow-worms due in part to the site receiving minimum maintenance and grounds keeping. Ecological Surveys of the site have also identified a badgers sett within the central area of the allotments site - as the badger sett is not directly impacted by the proposed works to the Garden Centre, the works are to be covered within the parallel application in relation to the Car Showroom.

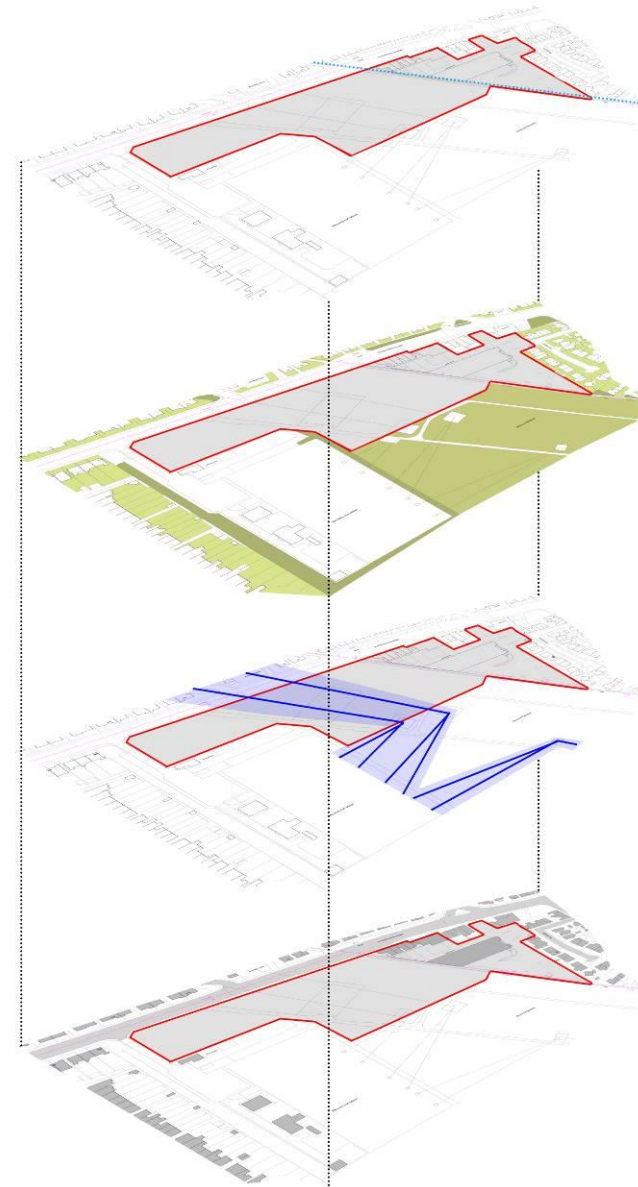


Image above: illustrative site constraint plans boundary (n.t.s.)

5.0 Amount of Development

The proposed development is the construction of an extension to the western end of the existing steel framed Garden Centre to create additional internal retail space and a dedicated 'goods in' warehouse to the south.

The existing offices and small storage area on the upper ground floor within the Garden Centre are re-located within the warehouse space and provide additional internal retail space. A schedule of accommodation is provided opposite detailing out the additional areas of development.

Externally the open air retail space is to be extended westward into the Eastbrook Allotments, which will also accommodate the 'goods in' yard and access to the warehouse. The yard has been designed to accommodate an articulated lorry.

The existing external retail area is partially covered with a series of frost canopies, which have been added in an ad-fashion over the years. The proposals include the demolition of the existinvg canopies and the construction of a new glass roof frost canopy which is be enclosed at the eastern end to provide a covered link between the Garden Centre and the existing café.

In order to facilitate the proposed development the existing PRoW needs to be diverted as part of the application. The proposed PRoW diversion is detailed in a later section.

In order to create the new 'goods in' area a new junction is proposed off Old Shoreham Road with a roadway leading from the main road to the warehouse. The proposed junction is detailed further in the accompanying Transport Statement by Motion.

The rationale behind the new 'goods in' yard is to reduce the number of deliveries currently being taken internally by the company, as it is currently not possible to have large vehicle deliveries to the Garden Centre due to the car park layout and change in levels. Therefore, at present deliveries are made by smaller vans shuttling between the Garden Centre at Hassocks

Existing Floor Area Schedule:

Existing Internal Floor Areas:

Retail Floor Area -	1167.8 m ²
Customer toilets -	24.6 m ²
Offices/ Staffroom/ toilets -	94.5 m ²
Store/ vertical circulation -	101 m ²
Lower ground store -	1173.1 m ²

Overall total: 2561 m²

Existing External Areas:

External Retail Space -	1137 m ²
Storage Area (upper gd)	239 m ²
Storage Area (lower gd)	464 m ²

Overall total: 1840 m²

Proposed Floor Area Schedule:

Proposed Floor Areas

Retail Area/ customer toilets -	1735.4 m ²
Offices/ staffroom/ toilets -	129.6 m ²
Warehouse -	457.4 m ²
Lower ground store -	1201 m ²

Overall total: 3111.4 m²

Proposed External Areas:

Glass house -	317.7 m ²
Goods In Yard -	490 m ²
External Retail Area -	2066.7 m ²
Storage Area (upper gd)	239 m ²
Storage Area (lower gd)	464 m ²

Overall total: 3577.4 m²

total additional floor area: 2197.8 m²

6.0 Layout - proposed extension

The proposed scheme design has been influenced by the existing building lines created by the Garden Centre, with a step in the plan form of the internal retail space being created from the 3.0m exclusion zone around the existing Southern Water mains drainage run, which runs parallel with the northern façade of the Garden Centre.

The existing internal retail space comprises of a large open floor, which is to be extended, with the existing offices, store area and staff welfare facilities re-located to open up the western end of the building. A new doorway is to be provided within the western gable wall to create a natural flow of customers through the internal retail space out to the external retail area.

The offices are to be re-located within the proposed warehouse/ 'goods in' area. The warehouse is to have direct access in the retail area to allow the distribution of stock. A new goods lift is to be installed at the junction of the extension and the existing building to provide access to the existing lower ground storage area and to replace the current lift.

The goods yard has direct access to both the warehouse and the external retail area, which again allows easy transfer of stock from storage to the retail spaces.

The external retail space is to be extended to the west of the Garden Centre and provide much needed additional retail space. The existing frost canopies, comprising of ad-hoc tunnel canopies, are to be demolished a new glass roofed frost canopy is proposed to partially cover the external retail area between the Garden Centre and the Pet Shop. The eastern end of the frost canopy is to be enclosed with glazing to provide additional protection to plant stock as well as an enclosed route to the café, which utilises the existing external doors.

There are no proposed changes to the Centre entrance, customer toilets or till area.



Image above: proposed upper ground floor extension (n.t.s.)

7.0 Scale - proposed extension

The proposals have been designed in consideration of their immediate context, specifically in relation to the plan position and heights of the adjacent buildings.

As the proposals are an extension of the existing building, the extension has been designed to match the existing ridge and eaves lines and detailing to respect the existing scale and massing of the Garden Centre.

The warehouse, which projects southwards from the existing Garden Centre building has been designed in width to allow the continuity of the eaves and ridge levels, therefore not altering the visual scale or massing of the built form.

The proposed extension is located away from the neighbouring properties so that there is no material impact on the amenities of the neighbours by overshadowing, nor overlooking.

The proposed frost canopies have been designed to not exceed the height of the existing frost canopies, therefore rationalising the roofscape of the Garden Centre without having any detrimental effect on the residents of the flats over the pet shop.

8.0 Landscaping - proposed extension

The proposal seeks to retain and extend the existing hard landscaping to the external retail area, which is to be populated with plant/ garden stock POS.

The external retail area is to be enclosed along the western boundary with security fencing with integrated vertical timber boarding to provide a secure boundary which is softened with the timber boarding.

The PRoW diversion is described in detail below.

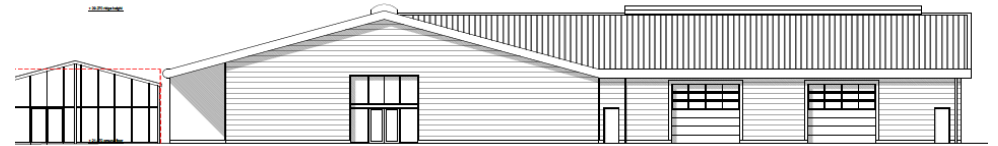


Image above: proposed western elevation of extension and warehouse

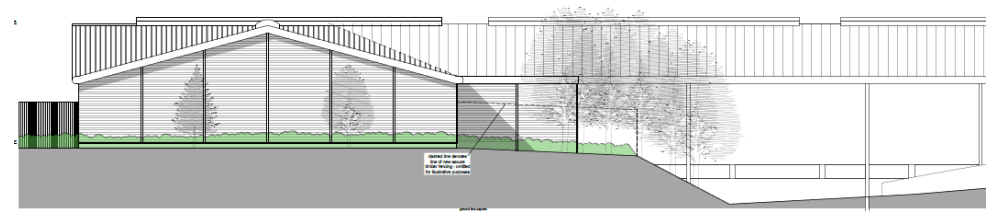


Image above: proposed southern elevation of extension and warehouse

9.0 Appearance - proposed extension

As described above the scheme has been designed to reflect and respect the existing levels and form of the surrounding area, taking into account the plan position of the structures as well as their respective heights within the site.

The materials have been chosen mainly for their visual appearance in providing a continuity of appearance, as well as being cost effective.

The roof is to be finished with insulated metal panel panels of a profile and colour to match the existing, with ridge line rooflights to maximise the levels of daylight within the internal volumes. The eaves are to be finished with polyester powder coated aluminium fascia system to match the existing.

The external walls are to be finished with horizontal timber affect fibre cement cladding boards which provide a robust finish to the external envelope, which would require minimal maintenance.

10.0 Access - proposed extension

The scheme, has at all times, been developed with the regulations regarding access and fire at the forefront of the design process, ensuring that all aspects are integrated into the scheme to create a holistic approach to the design.

The access into the site is to be via the existing vehicular entrance to the existing car park. There are no proposed changes to the public car park areas.

The proposals are classified as extensions to the existing buildings and have been designed to match into the existing floor levels where possible.

The facilities provided within the proposals have been designed to ensure access is provided for all users, which compliments the existing access arrangements to the garden centre.



11.0 Transport & Highways Summary

The transport strategy for the development builds upon the sustainable location of the site which is created by the extensive public transport provision available locally from the bus routes along Old Shoreham Road, through to Fishersgate Train Station to the South. There are excellent cycling and pedestrian links between the site and surrounding residential areas and public transport interchanges.

A new vehicular access is to be created into the allotment site off the Old Shoreham Road, which will serve the service area for the Garden Centre. The junction has been carefully designed and pedestrian refuge has been proposed to maximise the safety of pedestrians along Old Shoreham Road.

The internal layout of the site has been carefully designed to provide a parking provision in line with local guidance and will provide parking for staff for the garden centre, which will increase the number of customer parking spaces available at the Garden Centre.

Please refer to Motion's Transport Assessment for further details.

12.0 Sustainability Summary

In order to accord with Adur District Councils Sustainability Policy and the increasingly urgent climate emergency, detailed Sustainability Statements and Energy Calculations have been prepared as part of the design process and several measures have been introduced into the scheme to provide a minimum of 10% reduction in CO₂ levels.

This reduction is to be achieved, and intentionally exceeded, via a high standard of specification for the building fabric, doors, windows, electrical systems and the inclusion of photovoltaic panels on both the car showroom and the garden centre roofs.

Rainwater is to be collected in Rainwater Harvesting Tanks and utilised within the scheme including, but not limited to, flushing toilets and within the irrigation system of the garden centre.

The large paved areas will form a Sustainable Urban Drainage System (SUDs) via the specification of permeable paving which will negate the need to discharge surface water into the existing mains drainage system.

With the increase in prevalence of electric vehicles on our roads, a network of electric vehicle charging points is to be provided within the scheme, with additional infrastructure installed to allow a rapid expansion of the electric vehicle charging provision.

13.0 Acoustic Summary

A detailed acoustic survey has been undertaken on the site to provide background noise levels which have been used to analyse the acoustic impact of the proposals. The relocation of the Garden Centre delivery point has led to a reduction in the noise levels within the Garden Centre car park, resulting in an improvement to the living conditions experienced by residents on Park Crescent.

14.0 Ecological Enhancement Summary

In preparation for the forthcoming Planning Application a Preliminary Ecological Appraisal has been undertaken on the site. This concluded that the grasslands and common vegetation species offer 'limited botanical value'. More in-depth surveys identified a population of reptiles and badgers on the site, which are both classified as protected species, and an extensive mitigation strategy is being proposed to protect them.

The overall principle is to retain the biodiversity on the site with the creation of a new badger sett and areas of new planting to support both the badgers and reptiles populations on the site. Working closely with the Local Authorities a number of the reptiles are to be translocated to a receptor site which has been specially designed and managed to support the reptiles. A range of ecological enhancements are proposed across the site, including bat and bird boxes, a bio-diverse green roof on the car showroom, and the specification of native trees and planting.

15.0 Public Right of Way Diversion

The proposed expansion of the garden centre will require the diversion of the Public Right of Way (PRoW) to the West of the existing route. The footpath at present is about 1 metre wide, with 2 metre high mesh fencing on both sides and with extensive overgrown foliage encroaching into the footpath, and no artificial lighting. This creates an oppressive and potentially dangerous route at night time.

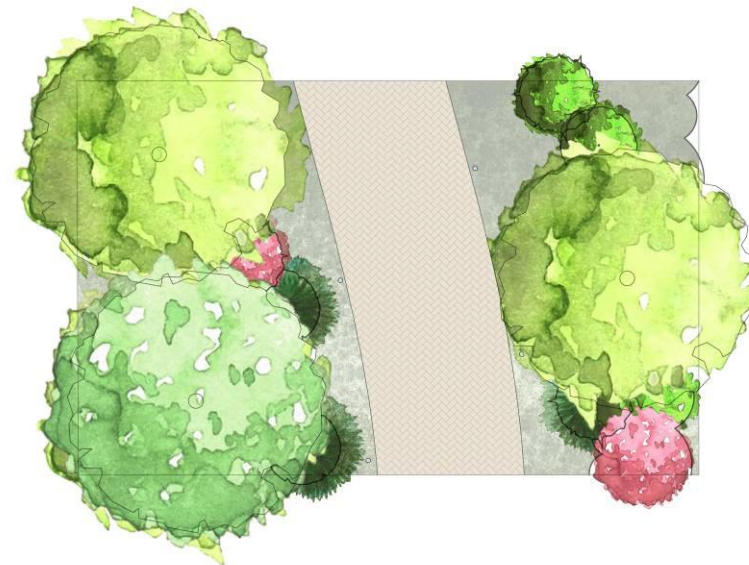
The Northern end of the footpath, accessed off Old Shoreham Road, runs South for approximately 230m, where a stepped access to/ from Stanley Road interrupts the footpath. The footpath continues on for a further 200m South to Manor Hall Road, with a couple of other access points from the adjacent residential streets.

The footpath takes approximately 11 minutes, at an average of 1.5m/s walking pace, from Old Shoreham Road to Manor Hall Road. The proposed diversion would add on an additional 59 metres to the overall length and about 1 minute to the overall travel time.

The new stretch of footpath would be 3 metres wide, with bollard lighting at regular intervals and comprehensive planting, including new trees, to provide some shade and interest to the route next to the garden centre. The footpath surface would be level and well drained which would greatly improve the accessibility of the footpath. Where the footpath turns the corner to the South of site, the path meanders through the new green buffer zone which forms part of the Ecological Uplift Strategy for the scheme.

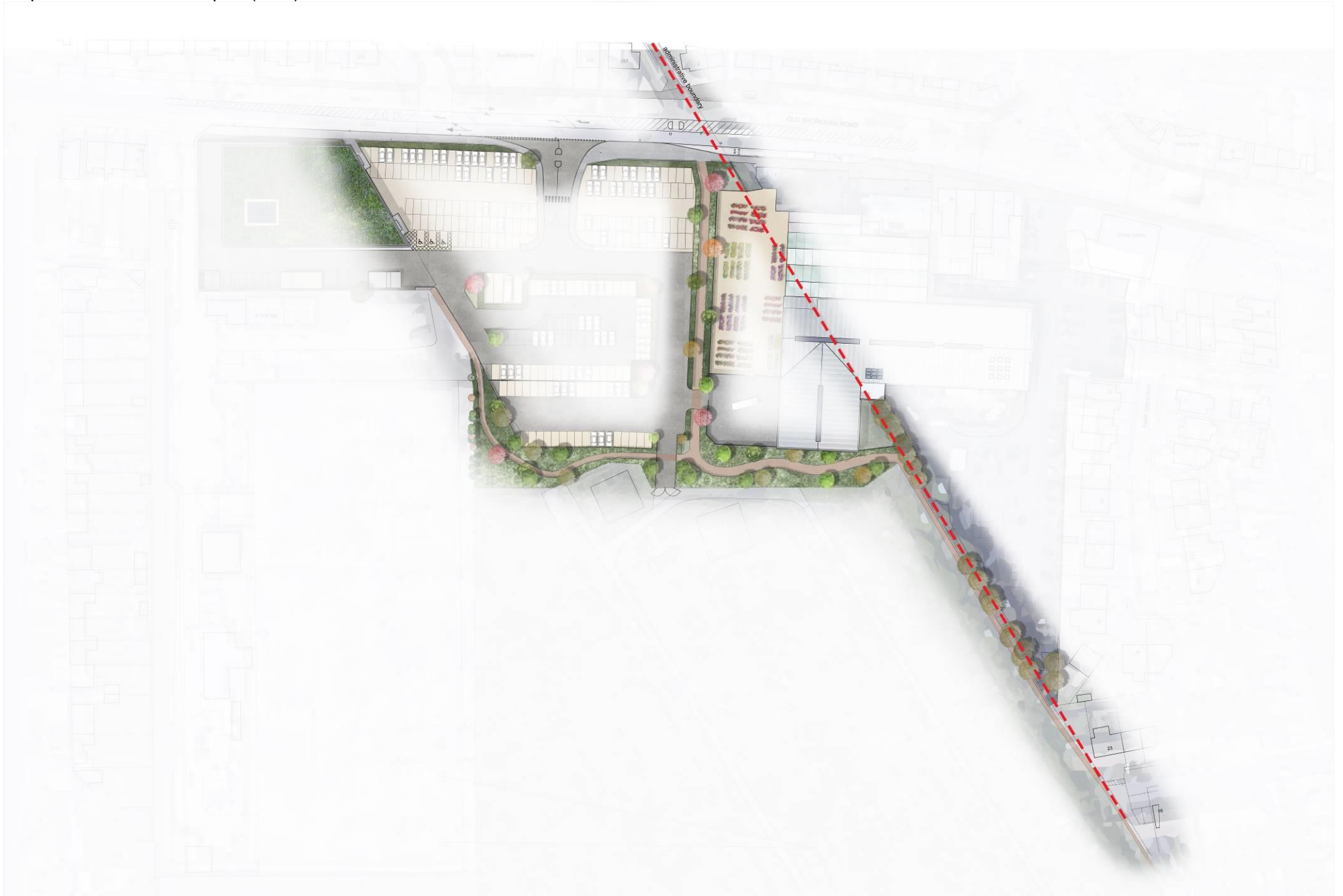
The footpath, would then return to its previous route to the East of the allotments, at which point additional lighting will be provided for the length of the garden centre car park.

A multi-disciplinary approach has been taken with the design of the footpath to ensure the experience is enhanced through the provision of an open and attractive route with good natural surveillance and passive measures of security as set out with the Secured By Design guidance.



Proposed cross section and plan detail and diverted PRoW

Proposed PRow diversion plan (n.t.s.)



Appendix A: Concept Visualisations

Concept imagery of PRow diversion and Garden Centre Extension



Concept imagery of PRow diversion and Garden Centre Extension

