



Construction Traffic Management Plan

For

Land North of Malting Lane, Braughing,
Ware, Hertfordshire

Version 1, July 2023

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Introduction

This document is intended to act as a working document outlining the traffic management proposals for the construction of properties and related works at Land North of Malting Lane, Braughing. The contents will be developed in tandem with the project construction programme to ensure that safe systems of work for traffic management on the site and adjoining roads are maintained.

The key areas identified in the document will be reviewed by the site team on a regular basis and with subcontractors prior to the commencement of their works on site and updated as necessary.

All site personnel will be informed as to the changes as they occur, this will be by means of information and posters in the site welfare.

Work Programme

The programme of works will commence as soon as possible.

Works associated with forming the new site access and S278 works in general will be conducted first, including the remodelling of traffic islands, changes to junction priorities and establishment of visibility splays.

A haul road finished to tarmac binder course will then be formed to allow clean access and egress from the site.

Units will be constructed broadly as a single phase, albeit with plot 1 left until the end to allow for better site management.

Drainage and service connections will form the early part of the programme.

The shared access road wearing course will be one of the final operations.

The anticipated programme duration from commencement of access works to site completion is 18 months.

Service Connections

Works in the highway will be required for mains water connections and telecommunications. Foul drainage and power connections can be achieved without works in the highway. We have been in contact with Affinity Water and Openreach and will co-ordinate their work to occur during our own road space application for the Section 278 works. At the time of producing this document, the Section 278 application is still under consideration by the Highways Authority (ref R5903573).

Local Consultation

It is Logan Homes' policy to contact homeowners and businesses neighbouring our sites to introduce ourselves and to give them a contact within our business should they need to raise any issues or have any concerns. We will conduct this exercise in the weeks before construction starts on site.

Neighbouring Construction Sites

From available information, there are no other active construction sites in the village of Braughing. The nearest other sites are on the outskirts of Bishop's Stortford and Clavering, neither of which will affect or be affected by this development.

Construction Site Layout

See below and 1:400 plan '7487BaughingSitePlan-ParkingStorage.pdf' submitted separately.



Parking

The site layout shows areas dedicated to contractor parking for up to 14 vehicles which will be sufficient to allow all parking to be within the site. Each space is appropriately dimensioned at 2.5m x 5m with at least 6m reversing space.

Estimated numbers of vehicles on site during various phases;

<u>Operation</u>	<u>Contractor vehicles</u>
Groundworks	3 (2 x groundworker, 1 x site management)
Superstructure shell construction	8 (4 x bricklayer, 2 x groundworker, 2 x site management)
First-fix operations	10 (2 x bricklayer, 2 x M&E, 2 x carpenter, 2 x plasterer / roof tiler / other, 2 x site management)
Second-fix and finishes	10 (2 x M&E, 2 x carpenter, 2 x decorator, 2 x landscaper / stats / other, 2 x site management)

If at any point during the works space on site is restricted and there is limited parking space, contractors who are able to access the site by alternative means such as public transport or by car-sharing will be asked to do so and deliveries will be carefully coordinated to avoid parking overspill from the site.

Procedure for Deliveries and Muck-away

The largest vehicles to use the access will be aggregate wagons, muck-away wagons, and grab lorries. All will be able to enter the site, turn and exit in forward gear. During excavation and pouring of property foundations, it is estimated that there could be up to 8-10 large lorries per day entering and leaving the site.

All loading and unloading of vehicles will be conducted wholly within the site. As shown on the construction site layout drawing, all vehicles will be able to enter, turn and exit the site in forward gear.

Instructions on the correct procedures will be given to companies who will be delivering or collecting from site and they will also be instructed to contact the site manager to advise on their arrival time. The site management team will be responsible for co-ordinating delivery times to avoid clashes. They will also be responsible for assisting safe access and egress, to police and ensure that traffic congestion is relieved on the site and in the surrounding roads.

Heras fencing used to secure the entrance overnight will be moved to one side to leave the access open throughout the working day.

Construction and delivery vehicle movements will be restricted to between the hours of 7:30 and 17:00 Monday to Friday (excluding bank holidays) and between 8:00 and 13:30 on Saturdays.

Lorries delivering concrete to site will be scheduled to be at least 1-hour apart from each other to ensure previous lorries are clear of the site.

Further Policies

- No delivery will be accepted on site without contractor personnel to unload them.
- No rubbish is to be left in the unloading zones or it will be removed at the subcontractor's expense.
- All vehicle delivery drivers are to wear PPE once inside the delivery zone (PPE – hard hat, hi-visibility vest, safety boots).
- No children will be allowed into the site accompanying the driver – vehicles will be turned away.
- It is the subcontractor's responsibility to ensure that all lorries delivering to site with HIAB cranes are fully certified, both the machine and the driver. Failure to provide the correct documentation will result in the delivery being turned away.
- All site staff must always maintain visual contact with each other while vehicle movements take place.
- At all times the need for reversing of vehicles is to be kept to a minimum and under no circumstances whatsoever will REVERSING manoeuvres take place without physical guidance from a Banksman.

Vehicle drivers will not normally be expected to attend a site induction however they will be verbally advised of the need to wear PPE whilst on site upon entrance to the project. Compliance with all site rules is mandatory.

Highway Control Measures

As stated above in the work programme, the cross-over and access area will be formed as part of the first operation on site and the access road will also be formed with a bound surface to prevent as much as possible any vehicle wheels becoming heavily soiled.

However, if any vehicles leaving the site do have heavily soiled wheels, they will have their wheels cleaned by a hose / pressure-washer.

Should any mud or debris be taken onto the adopted public highway, the highway in the vicinity of the site will be swept within an agreed time frame as and when reasonably requested by an officer of the Highway Authority.

The condition of the adopted public highway and footpaths in the immediate vicinity of the property will be recorded prior to the commencement of works. Any damage to the adopted public highway and footpaths found to have been caused by activities directly related to the construction activities will be repaired in a timely manner at no expense to the Highway Authority.

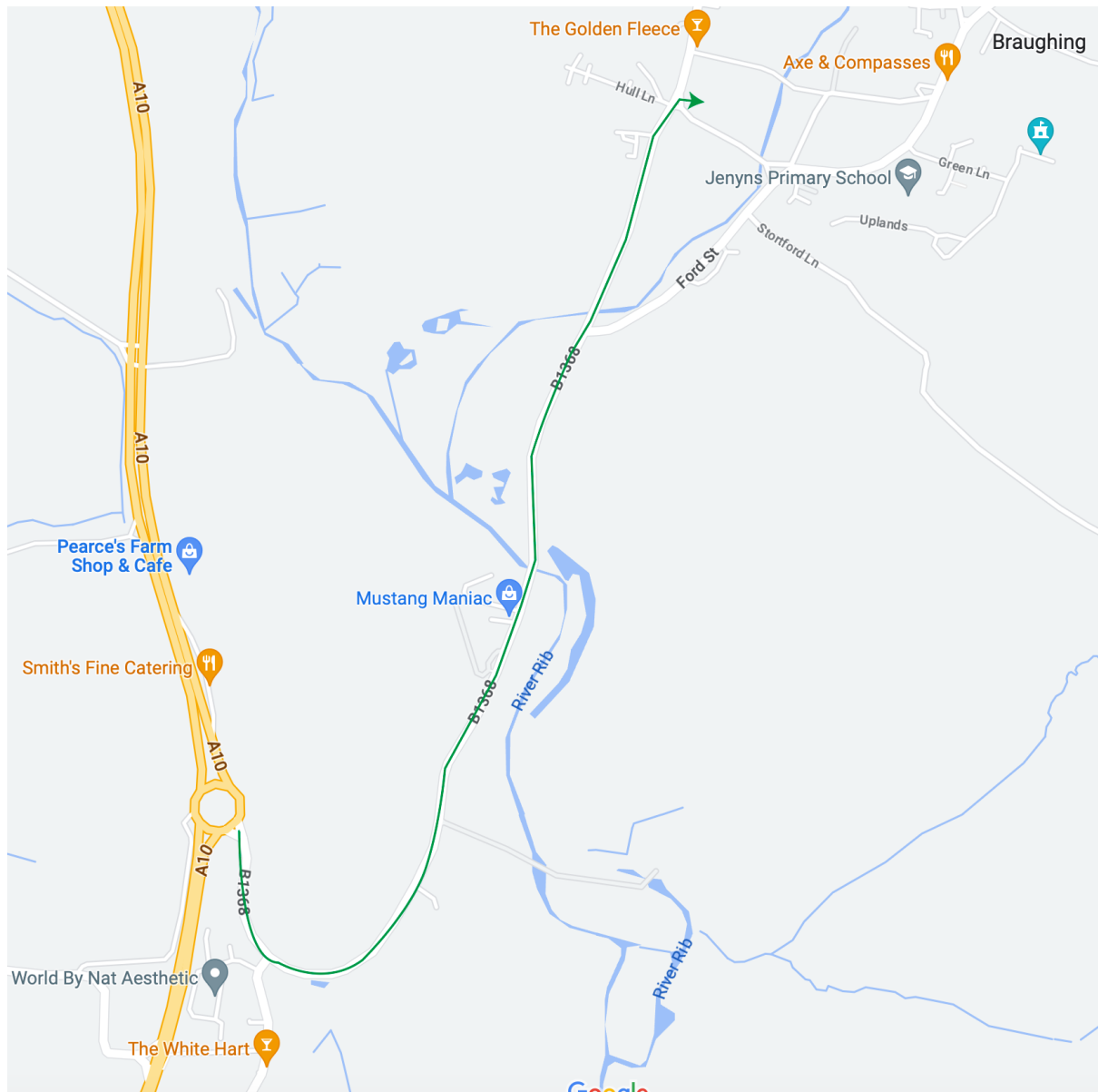
During construction activities that may create a substantial amount of dust, roads will be wetted down to prevent any damage to air quality and to prevent distribution of this dust on to the adopted public highway.

Vehicle Delivery Routes

A large majority of vehicles will access the site by turning off the Puckeridge junction of the A10 and proceeding North up the B1368, approaching the site from the South. The A10 has good connections to the wider road network.

Other vehicles will approach the site from the North on the B1368.

In particular, suppliers will be notified at the point of order not to make use of the narrow Horse Cross and Stortford Lane connection from the A120 to the village.



The site access has been subject to a Stage 1 Road Safety Audit and large vehicle manoeuvres demonstrating safe access and egress on to the B1368 have been approved by the Highways authority.

Hazards

Hazard Identification & Control Measures

Key hazard identified	Significant risk & who may be harmed.	Control measures.
Vehicles entering / leaving site onto Green End and Maltings Lane	Collision with pedestrians, employees, neighbours and road users including horses	Signage on HGV Entrance. Visibility splays to be maintained at all times. Vehicles to give way to pedestrians and road users at all times.
Restricted road and pedestrian access	Striking employees/pedestrians and temporary/permanent structures.	Vehicles to give way to employees/pedestrians at all times. Hi-visibility vests to be worn by all contractors and visitors.
Site personnel	Site personnel being struck by vehicles	Operatives wear high visibility garments whilst on site. All site personnel undergo a site safety induction before starting on site explaining the site rules.
Site visitors	Being struck by vehicles	Visitors must wear high visibility garments whilst on site. All visitors must be accompanied at all times.
Vehicles reversing in confined areas	Striking site personnel and other vehicles	Vehicles must have visual warning light on at all times. Vehicles must have audible alarm warning when reversing. Trained banksman will be in attendance at all times.
Drivers lacking competence	Out of control vehicles, striking site personnel and damage to structures	All site drivers to have a current certificate of training achievement and to be competent. Drivers to receive adequate supervision and monitoring.
Creation of mud and dust	Vehicles skidding on mud, drivers vision being impaired by dust	Vehicles to ensure that wheels are cleaned before leaving site. Any mud on road to be cleared up as soon as possible. Wetting down of site roads to reduce production of dust.

Monitoring and Review

Where specific hazards cannot be avoided, the risks they present will be minimised and controlled. Efforts will be made to avoid significant hazards before reliance is placed on risk control measures. The measures selected to control risks should be appropriate to prevent accidents on this site in each specific situation.

These control measures will have little effect unless they are constantly used; effectiveness is ascertained in practice and changes taken account of.

The procedures laid down will be monitored and reviewed every month against this assessment. The assessment will be updated as necessary to take into account the new hazards should there be any significant changes.