



NRP, MULTI-STOREY CARPARK EXPANSION

DESIGN & ACCESS STATEMENT

CARTER
DESIGN
GROUP





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Document Ref: 8657/LP/ID:	Revision: A	Issue Date: November 2023	Author: L Priestley
Reason for Issue: Information		Revision Note: Updated for final submission.	

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This Design and Access Statement has been prepared by Carter Design Group on behalf of Bullen Developments to convey the intent for the expansion to the Norwich Research Park (NRP) Multi-Storey Car Park, which is located on Rosalind Franklin Road, Norwich, NR4 7UQ.

This statement relates to the original Outline Application 2012/1880, Reserved Matters Application 2017/1197, and Non Material Amendment Application 2020/1332 and related discharge of conditions.

The 2017/1197 approved application was for a 1093 space car park to be constructed. The Non Material Amendment 2020/1332 application was approved for a 794 space car park to be erected with the option for future expansion.

This statement is to be read in conjunction with the following drawings submitted with this application:

- 8657.CDG.XX.XX.DR.A.0100.S4.P01 – Location Plan As Existing
- 8657.CDG.XX.XX.DR.A.0500.S4.P02 – Site Plan As Proposed
- 8657.CDG.XX.XX.DR.A.0550.S4.P02 – Block Plan As Proposed
- 8657.CDG.XX.00.DR.A.0900.S4.P01 – Ground Floor Existing GA Plan
- 8657.CDG.XX.01.DR.A.0901.S4.P01 – First Floor Existing GA Plan
- 8657.CDG.XX.02.DR.A.0902.S4.P01 – Second Floor Existing GA Plan
- 8657.CDG.XX.03.DR.A.0903.S4.P01 – Third Floor Existing GA Plan
- 8657.CDG.XX.RF.DR.A.0904.S4.P01 – Existing Roof Plan
- 8657.CDG.XX.00.DR.A.1000.S4.P02 – Ground Floor Proposed GA Plan
- 8657.CDG.XX.01.DR.A.1001.S4.P02 – First Floor Proposed GA Plan
- 8657.CDG.XX.02.DR.A.1002.S4.P02 – Second Floor Proposed GA Plan
- 8657.CDG.XX.03.DR.A.1003.S4.P03 – Third Floor Proposed GA Plan
- 8657.CDG.XX.RF.DR.A.1004.S4.P01 – Proposed Roof Plan
- 8657.CDG.XX.XX.DR.A.1200.S4.P01 – Existing Elevations
- 8657.CDG.XX.XX.DR.A.1201.S4.P01 – Existing Elevations
- 8657.CDG.XX.XX.DR.A.1210.S4.P03 – Proposed Elevations
- 8657.CDG.XX.XX.DR.A.1211.S4.P02 – Proposed Elevations
- 8657.CDG.XX.XX.DR.A.1212.S4.P02 – Proposed Elevations
- 8657.CDG.XX.XX.DR.A..1300.S4.P01 – Proposed Section A-A
- 8657.CDG.XX.XX.DR.A..1301.S4.P00 – Proposed Section B-B

SECTION TWO EXISTING CAR PARK

The existing site comprises of a 794 space car park facility over four storeys. A small kiosk provides accommodation for the car park attendants, including welfare facilities. Two accessible stair cores to the North of the site provide vertical circulation for pedestrians. This can all be seen in Figure 1 below.

The car park is accessed from Rosalind Franklin Road along the Western Elevation. Three queuing lanes are provided on entrance and have been designed to accommodate queuing traffic during peak times for the full complement of a 1093 space car park.

The exit of the carpark is located to the North of the site leading to Rosalind Franklin Road. Traffic is directed left on exit to avoid circulating round the Hospital perimeter road and the blue route for emergency vehicles.. Refer to Figure 3 for details. Again, peak flow of traffic is accommodated through provision of 4 queuing lanes on exit.



Figure 1 Existing MSCP - North East Elevation



Figure 2 Existing MSCP - North West Elevation

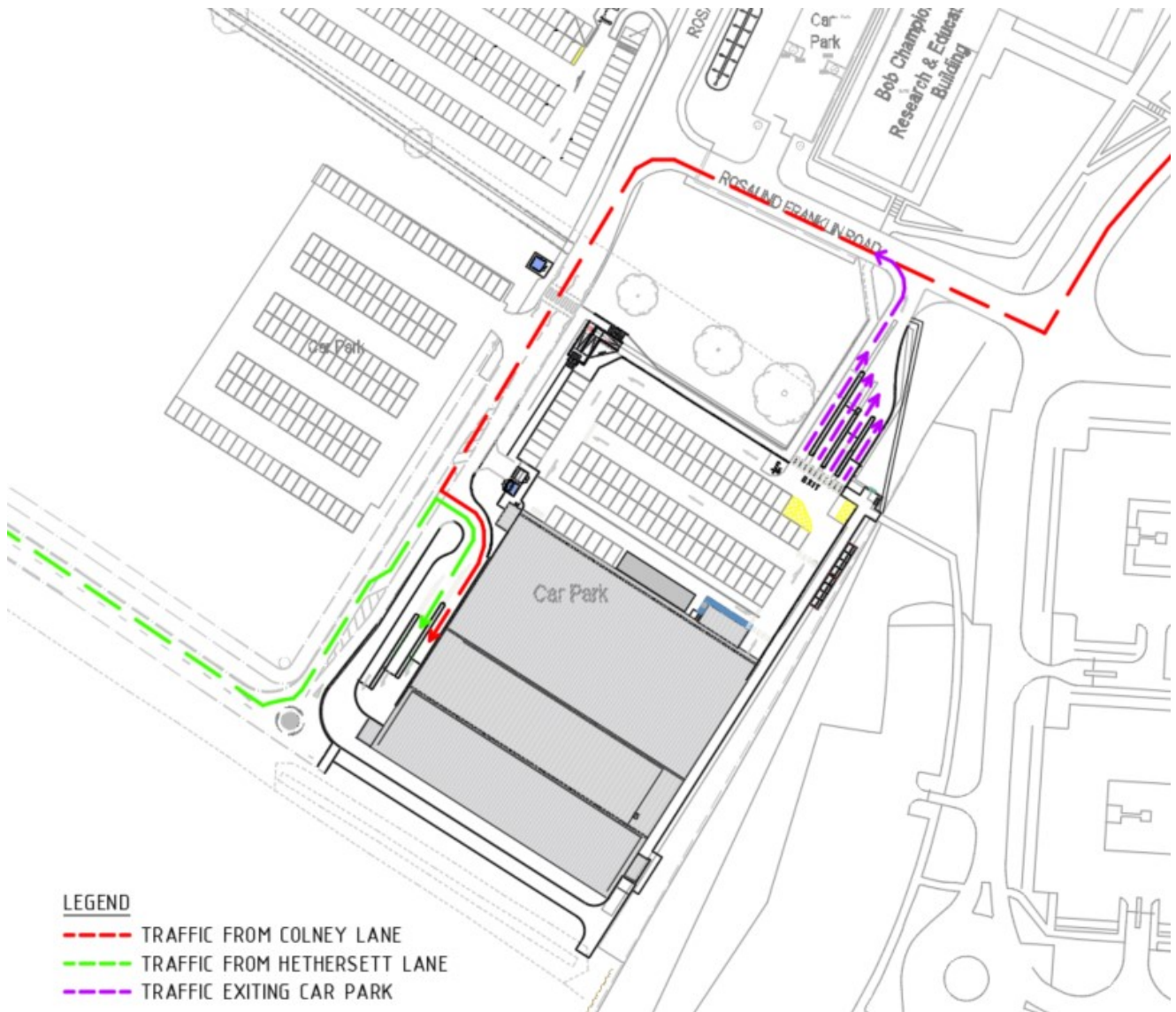


Figure 3 Vehicle Circulation

SECTION THREE DEVELOPMENT PROPOSAL

3.0 Development Proposal

The proposal is for an expansion to the existing Multi-Storey Car Park to the North East of the site, providing an additional 247 spaces to achieve a total of 1041 spaces over the 4 storeys, incorporating one additional stair in the North West corner within the red line boundary in Figure 4. The proposed extension would be constructed on the site and extent of the existing adjoining surface car park.

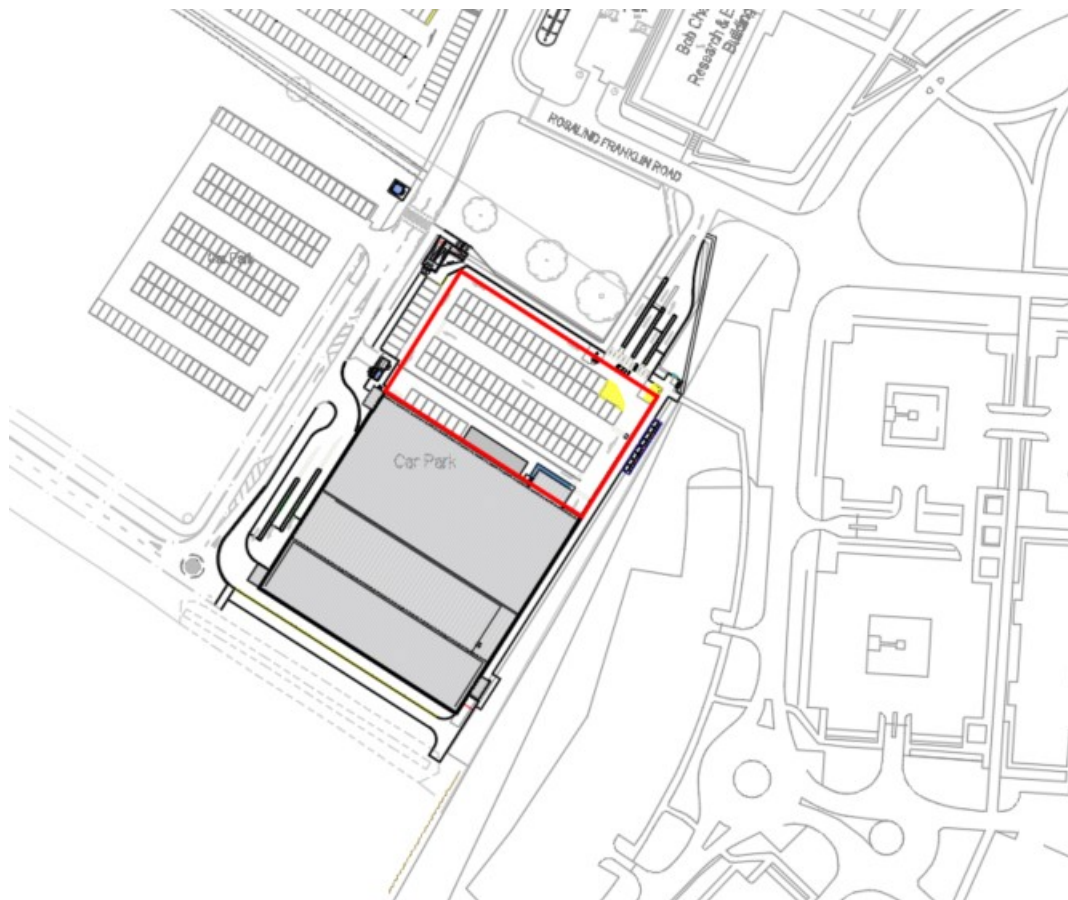


Figure 4 Red Line Boundary

3.1 Layout & Access

Vehicular access to the car park will remain unchanged and will continue to be from Rosalind Franklin Road which runs to the West. The three-lane entry point at the South East corner is to be retained and left unchanged as this allows for the full complement of spaces (1093) approved in application 2017/1197.

Vehicles will continue to circulate around the Ground Floor and each of the 3 upper decks, travelling vertically through the building by means of a single VCM ramp located centrally near the entrance of the car park. The layout of the car park is designed to make the transition to the upper decks and back down to the exit as direct as possible, whilst allowing vehicles to circulate around each deck freely to find a vacant space. The primary circulation routes are such that the flow of vehicles entering and exiting the car park are unimpeded.

Pedestrian access to the car park is currently provided from two primary access points located on the West and the North elevations. The proposal allows for an additional access point and stair core in the North West Corner. Two additional existing stair cores on South West & South East elevations are used for emergency egress only.

No accessible parking spaces are included currently or as part of the proposed extension. Persons requiring the use of a less able parking bay drive to their intended destination within the NRP site or the Hospital directly where dedicated less able parking bays are provided. Given the high volume of vehicle movements on the NRP/Hospital site it is not feasible to expect a less able person to park at the Car Park facility and safely navigate their way to their destination.

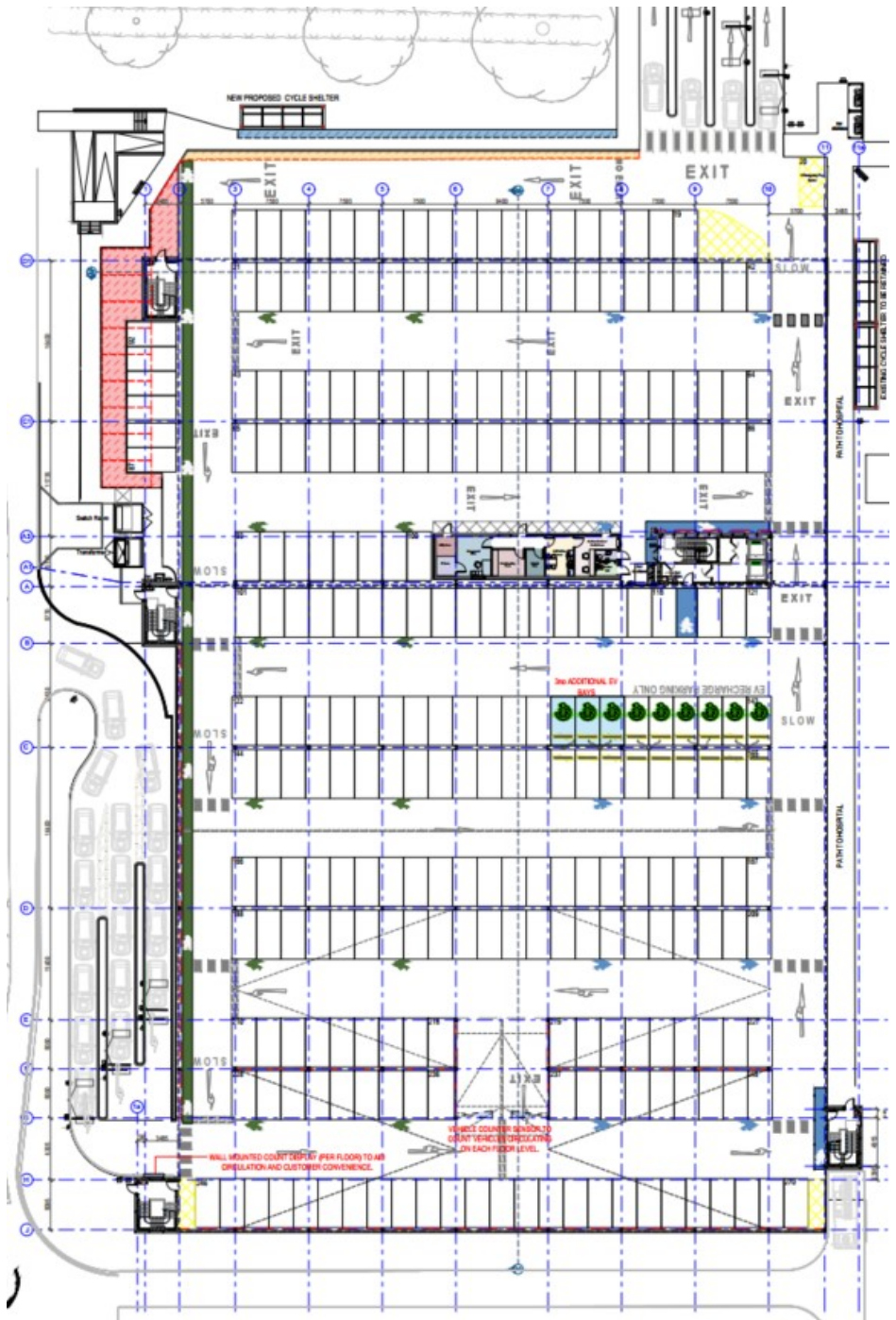


Figure 5 Ground Floor GA Plan - As Proposed

The car park exit route will also remain unchanged with vehicles exiting North onto Rosalind Franklin Road, with a no right turn in place to prevent vehicles circulating on the Hospital perimeter road and blue routes.



An accommodation block sits centrally within the car park. It is proposed that this remains in place, providing good means of access to the central stair core for staff, meaning any issues or emergencies on the upper decks can be efficiently reached. The kiosk is also adjacent to the main payment terminals meaning people requiring assistance are quickly dealt with. No stair core is proposed in the North Eastern corner of the car park, meaning users will still naturally circulate to the central core of the car park for vertical circulation as this is where persons on the upper deck are pointed to, containing the lift core.

Relocating the block to outside of the building footprint would have a negative impact on the Car Park user experience as well as being required to be relocated to the front edge of the site facing Rosalind Franklin Road, requiring existing landscaping and trees to be removed. The existing accommodation block is also importantly located adjacent to the core containing the sole services riser for the car park.

Existing cycle parking is located on the Eastern boundary of the car park. It is proposed that this provision is to be retained, with a new covered cycle store provided along the Northern boundary to facilitate key pedestrian linkages from the car park to the remainder of the NRP / Hospital site.

3.2 Appearance

The existing car park has been constructed using a long-span steel frame with concrete decks and ramps. The North, West and South elevations have been faced using vertical green fins set at varying angles to add visual interest and to vary the degree of visibility into the car park along the length of each façade.

The proposal is to use the same construction materials as existing, steel frame, concrete deck and fins to develop the proposed extension. The client wishes to consider incorporating a lighter colour into the extension which is yet to be determined. We would anticipate the imposition of a pre commencement condition to submit and agree material colours.

Refer to the proposed drawings below:

- 8657.CDG.XX.00.DR.A.1000.S4.P02 – Ground Floor Proposed GA Plan
- 8657.CDG.XX.01.DR.A.1001.S4.P02 – First Floor Proposed GA Plan
- 8657.CDG.XX.02.DR.A.1002.S4.P02 – Second Floor Proposed GA Plan
- 8657.CDG.XX.03.DR.A.1003.S4.P03 – Third Floor Proposed GA Plan
- 8657.CDG.XX.RF.DR.A.1004.S4.P02 – Proposed Roof Plan
- 8657.CDG.XX.XX.DR.A.1210.S4.P03 – Proposed Elevations
- 8657.CDG.XX.XX.DR.A.1211.S4.P02 – Proposed Elevations
- 8657.CDG.XX.XX.DR.A.1212.S4.P02 – Proposed Elevations



Figure 6 Stair core & Kiosk to remain in place.



Figure 7 Car Park entrance to remain in place

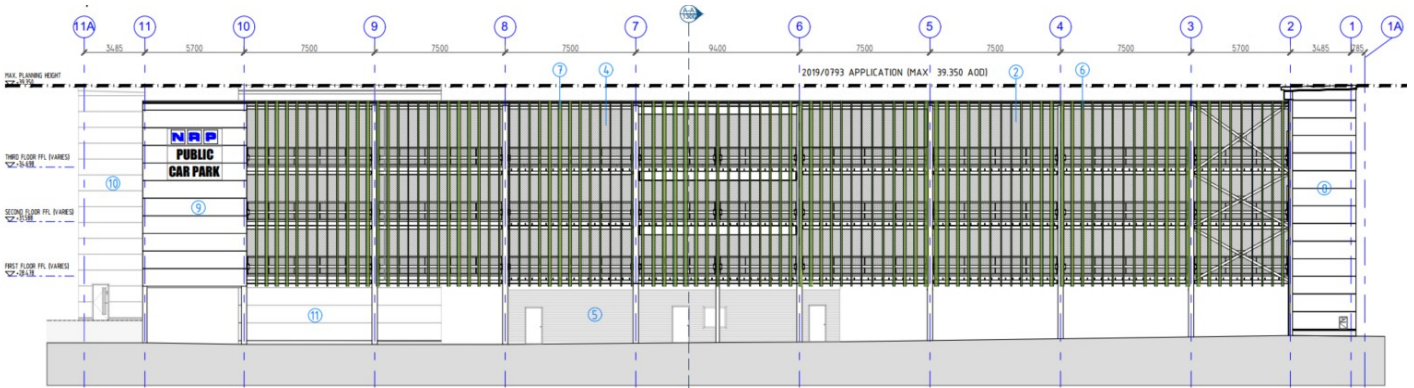


Figure 8 North East Elevation, As Proposed

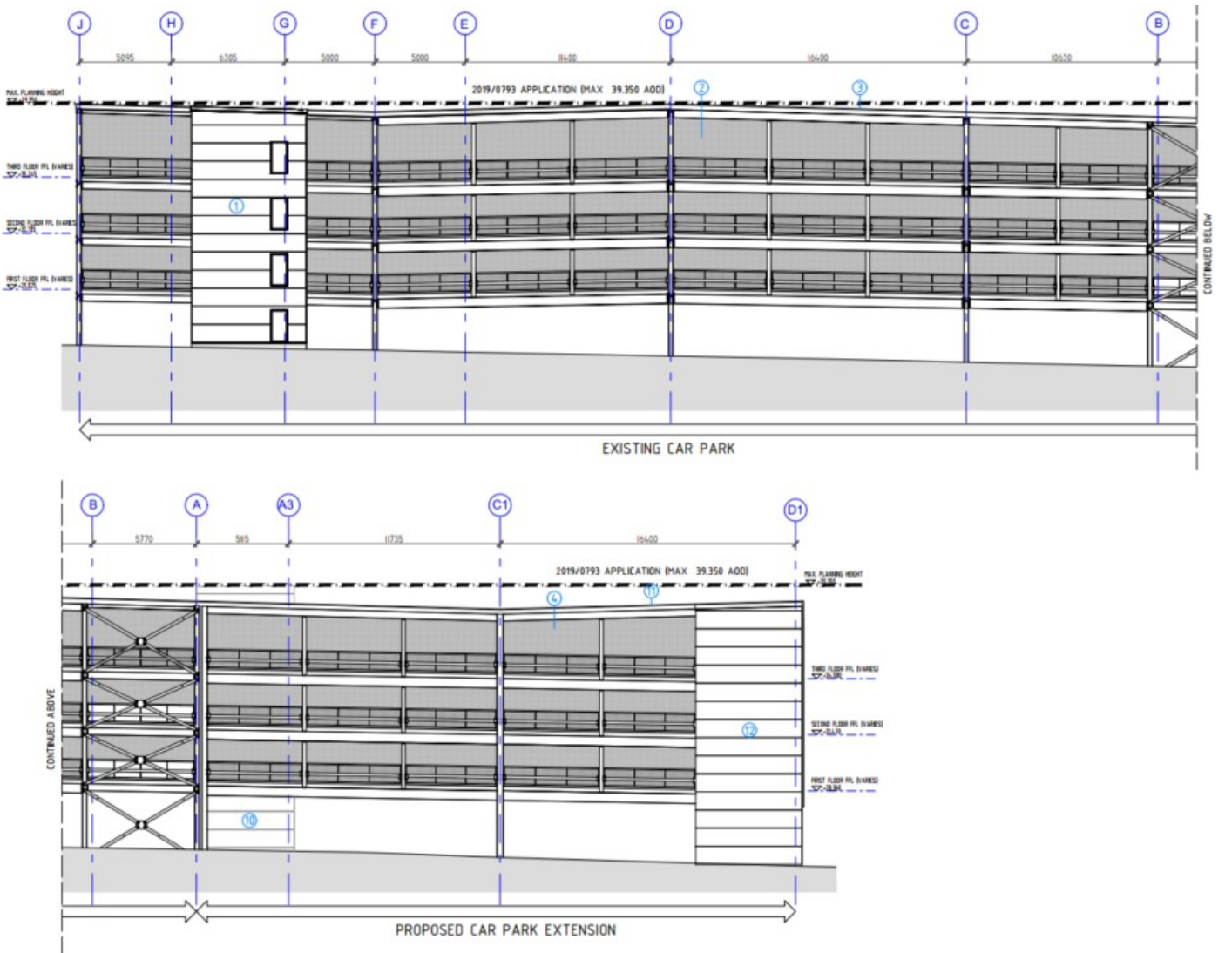


Figure 9 South East Elevation, As Proposed

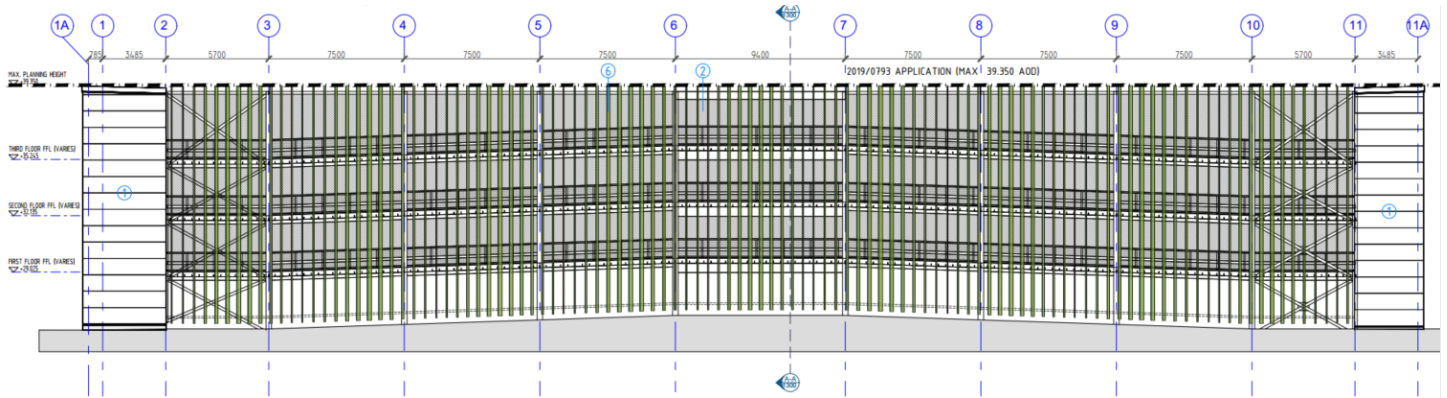


Figure 10 South West Elevation, As Proposed

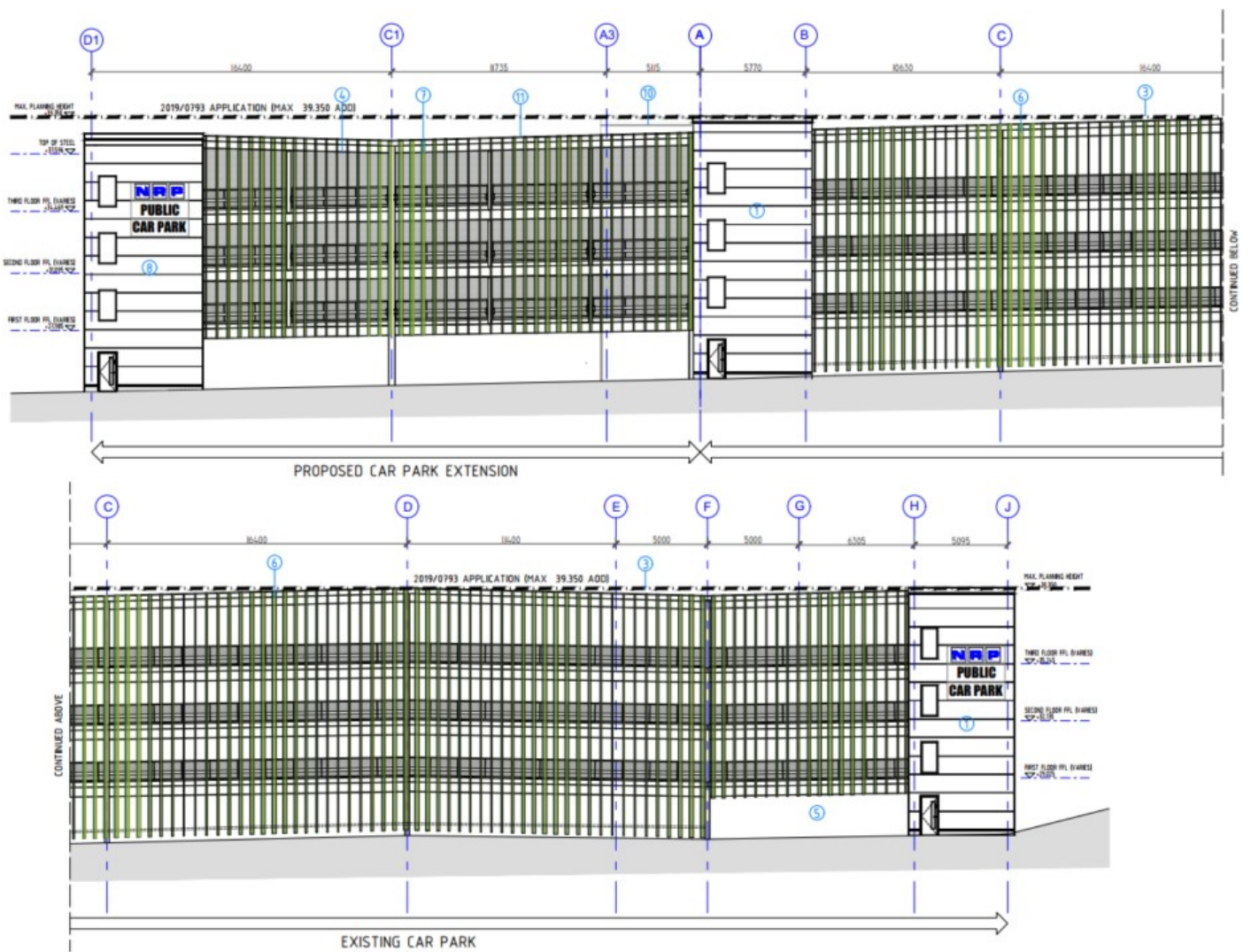


Figure 11 North West Elevation, As Proposed

SECTION FOUR CONSULTATION REPORTS

4.0 Supplementary Information

The design an access details have been prepared in light of previous reports submitted within earlier applications for planning permission and, where appropriate, updated in relation to the proposed extension. There reports include those assessing archaeology, ecology, arboriculture, flood risk and drainage and transport.

4.1 Flood Risk Assessment and Drainage Strategy Update

The site is located within Flood Zone 1, and remains at very low risk of flooding from all sources. The proposed development does not change any of the parameters set out in the Flood Risk Assessment and Drainage Strategy reference 7687/12/AMM/MSCP dated 15/05/17, approved under 2019/2408.

The proposed expansion to the current MSCP extends over an area of impermeable parking surface to the North of the existing structure as shown on drawing 8657.CDG.XX.XX.DR.A.0550. Surface water drainage systems were constructed in advance of the MSCP development in accordance with the approved strategy (2019/2408). The entire impermeable area generated by the existing car park facility (both building roof and paved areas) discharges surface water to ground via the existing soakaway design to accommodate 1/100yr storm return plus 40% climate change allowance. The proposed MSCP expansion does not seek to increase the existing impermeable area, and therefore no modifications to the existing soakaway provisions are deemed necessary.

The proposed MSCP expansion shall provide substantial improvements on water quality through the further segregation of 'clean' roof water from car park surface water. The proposed total roof area of 225m² shall drain directly to the existing soakaway utilising the existing roof water drainage network, whilst the residual open-air paved areas, along with the covered parking decks, shall drain via the existing full retention separator in a separate surface water network, replicating the existing provision. This existing separator will provide a significant degree of robustness given that it has been designed to accommodate a much larger impermeable area (open air parking).