

NRP MSCP Extension Transport Statement

Project number 60432496	Client name Bullen Developments Ltd	Subject NRP MSCP Extension	Date December 2023
Prepared by T Jones	Checked by G Whitehead	Verified by B Carey	Approved by C Brooks

Revision History

Revision	Revision date	Details	Authorised	Name	Position
1.0	22/11/2023	Draft Issue to client team	BC	B Carey	Regional Director
2.0	24/11/2023	Final Issue to client team	BC	B Carey	Regional Director
3.0	01/12/2023	Final Issue for Planning Submission	BC	B Carey	Regional Director
4.0	05/12/2023	Final Issue for Planning Submission – NNUH Statement	BC	B Carey	Regional Director

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1.0 Introduction

- 1.1 AECOM has been appointed by Bullen Developments Ltd to provide transport planning advice to accompany the proposals for the extension of the existing Multi-Storey Car Park (MSCP), situated at Norwich Research Park (NRP) in Norfolk. This Transport Statement (TS) has been prepared to accompany a full planning application.
- 1.2 It is proposed to increase the number of spaces provided at the MSCP at NRP by approximately 250 to cater for the displaced / restricted demand created by the Norfolk and Norwich University Hospital (NNUH) adjacent to the MSCP and the requirements of occupiers who have come forward at the NRP. It is acknowledged the NNUH is part of Bullen Developments land strategy. The increase in capacity will result in the total number of spaces increasing from 794 to 1,041 spaces (an increase of 247 spaces). This is also to forward plan the demand for car parking whilst buildings come forward at a slower pace. All access arrangements for the MSCP are proposed to be retained as currently provided and Bullen will continue to fully operate this trading car park facility
- 1.3 The existing multi-storey car park was constructed based on the agreed illustrative masterplan for NRP South which sought to provide the majority of car parking for NRP occupiers with the remainder of operational parking provided on-plot as individual buildings are built. Additional short term requirements for car parking have arisen from the adjacent hospital and NRP.
- 1.4 When the MSCP was originally built, it was built on the site of a temporary car park serving NNUH. NNUH are experiencing overcrowding and reduced capacity within their existing surface car parks as they develop new facilities on surface car parking, which is impacting on their ability to operate effectively for staff and patients. NNUH and Bullen Developments have negotiated the provision of spaces in a designated area within the existing MSCP for NNUH use on a temporary basis. To allow for further capacity for NNUH, whilst retaining sufficient capacity to cater for NRP developments as they come forward, Bullen Developments are now proposing to expand the proposed car park to allow for long term capacity for NNUH use. The NNUH are direct occupiers of NRP Zone 3 and are expected to construct further buildings on Zone 3 over time which will require car parking.
- 1.5 Car parking related to any new NNUH buildings will be part of the relevant planning applications and, therefore, is not considered as part of this work. This application relates to parking spaces to cater for capacity for existing vehicles attending the hospital.

1.6 The location of the proposed development site, in context with the NNUH, is shown in **Figure 1** below.



Figure 1. Proposed Development Site Location

1.7 The structure of the TS is as follows:

- Introduction;
- Planning Background;
- Existing Transport Conditions;
- Existing and Proposed Development;
- Highway Impact; and
- Summary and Conclusion.

2.0 Planning Background

Planning History

2.1 There have been three reserved matters planning applications by Bullen Developments Ltd. for a multi-storey car park on this parcel of land, as part of outline planning consent 2012/1880. This car park is fully managed and operated by Bullen Developments. These three applications and the number of car parking spaces applied for are as follows:

- 2016/2382 – 742 spaces;
- 2017/1197 – 1,141 spaces; and
- 2019/0793 – 800 spaces.

2.2 All three reserved matters applications were approved by the planning authority, but only consent 2019/0793 has subsequently been delivered. The planning consent 2017/1197 for 1,141 spaces included a TS that assessed the local highway network and included junction modelling. This was accepted by the Highway Authority and planning authority and so the principle of increased parking has previously been assessed and agreed.

- 2.3 All NRP spaces within the MSCP are provided in line with the NRP Supplementary Planning document (SPD) providing an average parking ratio of 1:60 square meters of floorspace.

3.0 Existing Transport Conditions

Site Location

- 3.1 The development site currently exists as off-street parking (97 standard-size spaces) at ground level, as part of the wider NRP MSCP with the main multi-storey building immediately to the south of the site. There is a one-way system through the site with vehicles entering and exiting via Rosalind Franklin Road.
- 3.2 The site borders Rosalind Franklin Road to the west and north, the existing MSCP to the south, and the main NNUH staff car park to the east beyond a belt of trees.

Existing Walking Facilities

- 3.3 Near the site, there are existing pedestrian facilities along Rosalind Franklin Road parallel to the carriageway adjacent to the MSCP to the north, between the zebra crossing towards the Bob Champion Research & Education Building and the zebra crossing towards the Quadram Institute staff car park.
- 3.4 Furthermore, there is a footpath leading to the NNUH main staff car park to the southeast of the site, which provides pedestrian access to the hospital building.
- 3.5 There are additional walking facilities around the north-eastern, south-eastern, and most of the north-western side of the MSCP, as well as painted walking routes within the car park.
- 3.6 There are no Public Rights of Way (PROW) in the immediate vicinity of the development site.

Existing Local Highway Network

- 3.7 Rosalind Franklin Road runs east to west and provides access to the MSCP. At its western end, it intersects at a roundabout with Hethersett Lane, which provides access to Hethersett and Wymondham and other smaller settlements to the south and the B1108 Watton Road to the north. B1108 Watton Road runs east to west and provides access to the A47 to the west and Norwich city centre as B1108 Earlham Road / B1108 Unthank Road.
- 3.8 The exit of the MSCP is at a priority junction with Rosalind Franklin Road, located approximately 40 metres to the west of the NNUH access road, which provides further links with Colney Lane at the NNUH Access Road / Colney Lane roundabout. Only left-hand turns are allowed at the MSCP exit, as it is intended that wider highway network access from/to the MSCP is via Hethersett Lane rather than the NNUH perimeter road.

Committed Local Highway Network

- 3.9 Local off-site committed highway improvements will improve the capacity of the network in the immediate area.

4.0 Existing and Proposed Development

Existing MSCP

- 4.1 The existing NRP South MSCP provides a total of 794 spaces (slightly below the 800 spaces permitted) over four levels and an external surface parking area. It was built in 2020. These spaces are open to the public with some held under temporary licence to NRP occupiers and NNUH on parking permits and are open to the public. The public use is the predominant use. Ultimately the existing MSCP is to serve the needs of the NRP South development occupants as the sites are built out and occupied and Bullen Developments Ltd. intends to continue providing and operating this car parking facility.
- 4.2 There are 18 spaces equipped with charging facilities for electric vehicles. No parking for disabled users is provided within the MSCP as all disabled parking provision is being provided on-plot at the individual NRP buildings as they come forward at NRP South. At NNUH, disabled parking is located near building entrances.
- 4.3 Car parking charges for existing NNUH spaces within the MSCP are in keeping with that across the NNUH site.
- 4.4 Vehicular access / egress is via Rosalind Franklin Road, located to the west and north of the existing MSCP respectively. These provide separate entry and exit points.
- 4.5 The point of entry to the MSCP is from the west, via Rosalind Franklin Road, which consists of a priority junction leading into three-lanes for the barriers. Each entry lane provides space for approximately four vehicles to wait at each of the barriers without impacting on vehicle flow on Rosalind Franklin Road. A right turn lane is provided on Rosalind Franklin Road with space for three cars to reduce blocking back. Traffic can enter the MSCP on Rosalind Franklin Road from both the Hethersett Lane and Colney Lane directions.

- 4.6 The vehicular exit from the MSCP is directly onto Rosalind Franklin Road to the north of the MSCP via a priority junction. A four-lane exit to the barrier is provided to increase the number of vehicles which can exit the car park, reducing the level of stacking space required. The exit lanes provide space for approximately seven vehicles in total to wait at the barriers without impacting upon the circulatory flows of the car park. Exiting vehicles can only turn left at Rosalind Franklin Road, meaning all traffic is directed towards the Rosalind Franklin Road / Hethersett Lane roundabout, and no traffic is directed towards the NNUH or Colney Lane.
- 4.7 There are three pedestrian access points to the MSCP, each connecting to external footways on Rosalind Franklin Road and the NNUH perimeter road. At the car park blue signage is used to direct NNUH users to the nearest pedestrian exit for NNUH. There is a direct lit footpath that connects the MSCP to the NNUH perimeter road providing connection to the NNUH internal footways and crossings, delivering safe route to NNUH. Within the NNUH site, there are footways and marked crossing points for pedestrians to the MSCP building.

Parking Requirement

- 4.8 There is a statement of support as set out in the 'NNUH Statement of Support' in **Appendix A**.
- 4.9 As part of the development proposals, all 247 additional car parking spaces are to be provided specifically for NNUH usage. These additional car parking spaces will not represent any increase in car parking to serve the NRP masterplan. NNUH have confirmed that these spaces are required as a result of the following:
- Loss of parking in Car Park C (approximately 100 spaces) to accommodate a new building;
 - Overcrowding in their current surface car parks;
 - Unregulated parking in the plaza area which will be removed as part of proposed improvements to this area; and
 - Loss of general parking in Car Park D to accommodate disabled parking and drop off, intended to be displaced from the plaza area to support bus, taxi, and ambulance capacity.
- 4.10 Overcrowding of existing NNUH car parking spaces is a result of increased visitors over the years due to increased patient services, with no additional car parking being provided to cater for this increase. In addition, NNUH do not currently monitor or limit number of vehicles entering a car park when it is full, which results in overcrowding. The NNUH implement a Travel Plan on site whereby sustainable travel for visitors is promoted, however there is still a need for visitors and staff arriving by car to park on site.
- 4.11 There are no current proposals for NNUH to build their own car park on site as part of their master planning process.
- 4.12 The proposed increase in spaces are to be provided to cater for the existing parking demands of NNUH, which will continue to increase as spaces are lost on the NNUH site to deliver new buildings and increased capacity for sustainable travel.
- 4.13 The original plans for the MSCP were predicated on the assumption that it is provided to meet the bulk of the parking requirements for NRP South (east of Hethersett Lane) and not the requirements of the hospital. The NRP South buildings have not yet all been delivered. The requirements of the hospital have emerged since the granting of outline planning permission for NRP South and NRP's continuing growth. Without the increase in capacity of the MSCP there is no long-term certainty that this capacity will be available to the NNUH.
- 4.14 It is intended that existing leasing arrangements will continue between NNUH and Bullen Developments Ltd (the car park owner) for the required spaces in the MSCP.

Proposed MSCP Extension

- 4.15 The proposed development consists of a northern extension to the existing MSCP at NRP South. It will provide a new four level car park on the site of the current surface car park. The resultant net increase in spaces will be provided specifically for NNUH usage.
- 4.16 Car parking charges for NNUH will be the same across the NNUH site and the proposed spaces within the MSCP to ensure consistency. Payment options will be the same as currently in place.

4.17 **Figure 2** below illustrates the proposed MSCP extension. The site plans are included at **Appendix B**.



Figure 2. Proposed MSCP Extension

4.18 The existing and proposed number of car parking spaces at the MSCP is shown in **Table 1** below.

Table 1. Existing and Proposed Car Park Spaces

	Existing	Additional	Total
MSCP Spaces	794	247	1041

Source: Carter Design Group, NRP South Multi-Storey Car Park Extension GA Plan

4.19 All the additional spaces will be of standard provision. A total of nine existing standard spaces will be converted to EV spaces, these spaces will be next to the existing EV spaces.

4.20 The vehicular and pedestrian access arrangements are to be unchanged from the current arrangements to and from the existing MSCP.

5.0 Highway Impact

5.1 The number of car parking spaces as part of the proposed development is compared with the number of spaces that was consented as part of the 2017/1197 planning application in **Table 2** below.

Table 2. Existing and Proposed Car Park Spaces

Option	Car Parking Spaces	Difference to 2017/1197 Consent (Spaces)
2017/1197 Consent	1,141	0
As Built	794	-347
Proposed Development	1,041	-100

- 5.2 This shows that the proposed development is proposing 100 fewer total car parking spaces than was assessed and consented for in the 2017/1197 application. The TS for the 2017/1197 consent assessed the impact of a 1,141 space MSCP at the site and concluded that the NNUH / Colney Lane roundabout and the Hethersett Lane / NRP South Access roundabout would both operate within theoretical maximum capacity in the years assessed (i.e., 2026 Future Year).
- 5.3 As the number of parking spaces proposed at the MSCP continues to be below 1,141 spaces, the conclusions previously made in relation to the operation of the NNUH / Colney Lane roundabout and the Hethersett Lane / NRP South Access roundabout remain valid. It has therefore not been considered necessary to re-assess the junctions.
- 5.4 It is noted that the MSCP as built does have some differences over that proposed in the 2017/1197 consent in terms of the number of entry and exit lanes. These changes are within the same footprint for the previously permitted scheme and do not impact upon local junctions. The increased number of entry and exit lanes represents an improved situation for access and egress for those using the car park, reducing potential for delay, and blocking back on the local road network and within the car park.
- 5.5 Given the above, it is considered that the increase in car parking spaces will not have a detrimental impact on the highway network.

6.0 Summary and Conclusions

- 6.1 It is proposed to erect an extension to the existing MSCP at the Norwich Research Park South. As part of the extension, 247 additional standard spaces are proposed for NNUH use.
- 6.2 The site currently serves as off-street parking at ground level to the north of the existing MSCP. It boasts a variety of walking facilities to the surrounding area, with a direct lit footpath to the NNUH site. The site is also well-connected to the wider highway network.
- 6.3 The need for the extension is motivated by overcrowding in the current NNUH car parks, and car parking being lost as a result of building construction and sustainable transport developments on the NNUH site.
- 6.4 The additional car parking spaces proposed as part of the MSCP extension will serve existing NNUH car parking demand and enable the implementation of planned developments at NNUH to proceed, allowing the NNUH to operate and expand efficiently.
- 6.5 As the MSCP expansion is proposing 100 fewer total car parking spaces than in the 2017/1197 consent, it is considered that the proposed development will not have a detrimental impact on the highway network.

Appendix A – NNUH Statement of Support (To Follow)

Appendix B – Proposed Site Plans

NRP SOUTH, MULTI-STOREY CAR PARK EXTENSION

CAR PARK SPACES				
	EXISTING SPACES	ADDITIONAL SPACES	TOTAL SPACES	DECK AREA (M2) PER ADDITIONAL SPACE
GROUND FLOOR	269	1	270	N/A
FIRST FLOOR	174	82	256	26.4
SECOND FLOOR	174	82	256	26.4
THIRD FLOOR	177	82	259	26.4
TOTAL:	794	247	1041	26.4

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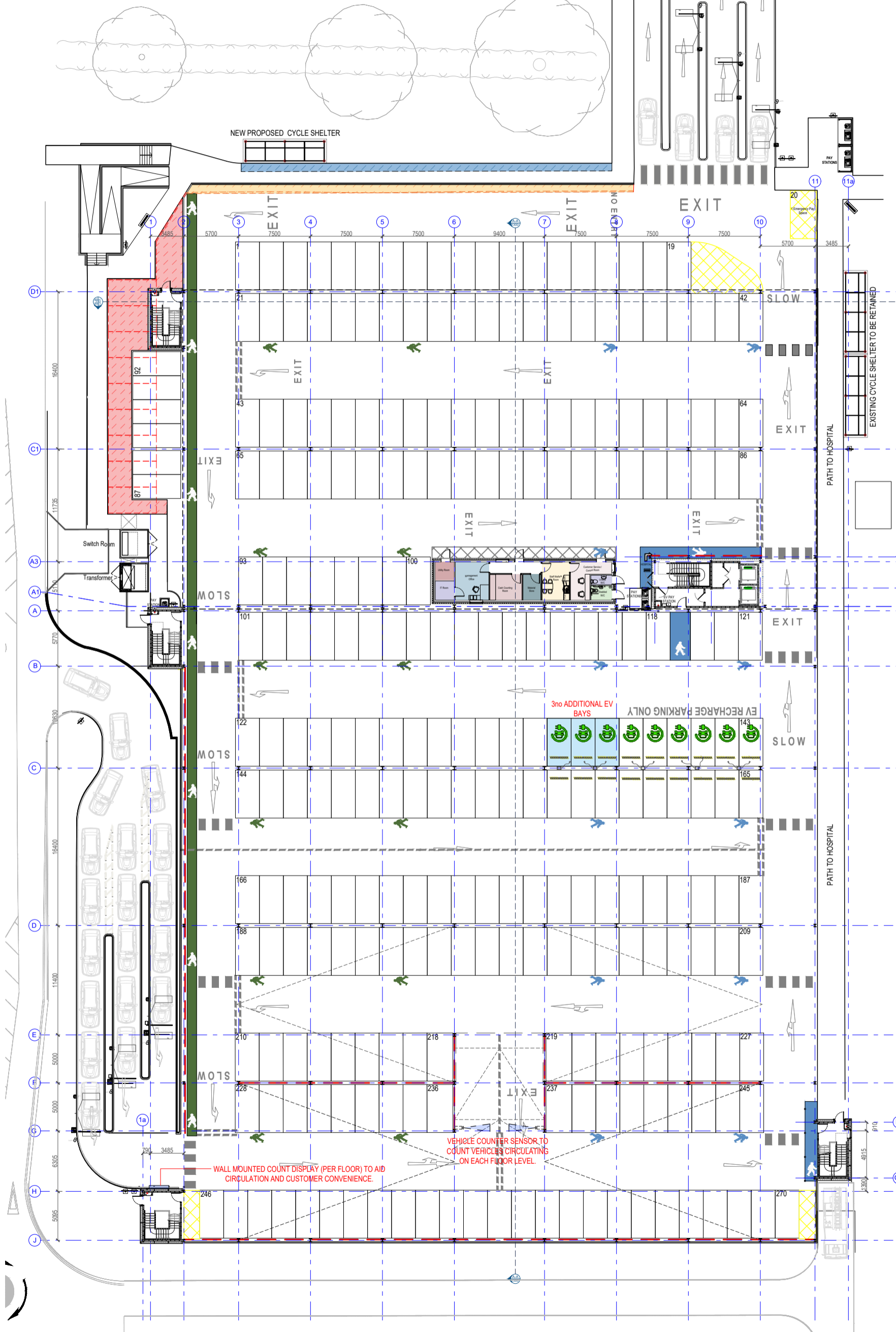
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LEGEND:

- DENOTES EXISTING LANDSCAPING AREA TO BECOME FOOTPATH.
- DENOTES EXISTING FOOTPATH TO BECOME ASPHALT PARKING AND WALKWAY.
- DENOTES EXISTING ASPHALT PARKING TO BECOME FOOTPATH.



54.P02	Additional cycle shelter included & cladding replaced with blockwork around central stair core.	2011.2023	RGC	LP
54.P01	North Point added	29.06.2023	CDG	AG
no	revisions	date	source	drawn

PLANNING

NRP SOUTH, MULTI-STOREY CAR PARK EXTENSION
PROPOSED GROUND FLOOR GA PLAN

drawn	LP	checked	AG	scale	1:200 @ A1	project	number	revision
date	05.06.2023	date	05.06.2023			8657.CDG.XX.00.DR.A.1000	S4.P02	

PROPOSED GROUND FLOOR GA PLAN SCALE: 1:200 @ A1

VICTORY PARK, LONDON ROAD, ATTLEBOROUGH, NORFOLK, NR11 2DA. TEL: 01553 767788 E-MAIL: enquiries@cdg.co.uk

NRP SOUTH, MULTI-STOREY CAR PARK EXTENSION

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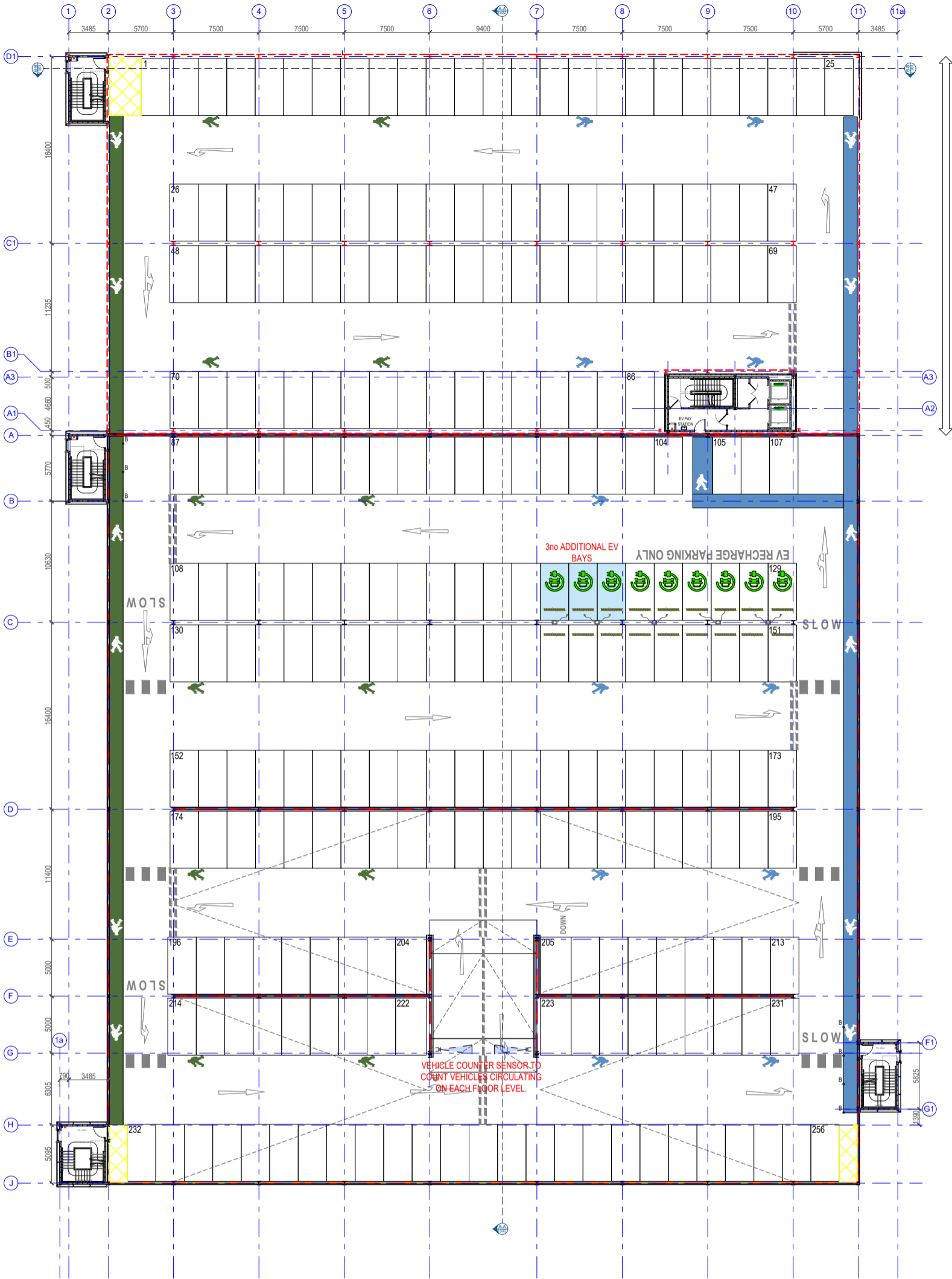
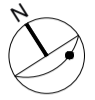
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TOTAL:	794	247	1041	26.4

S4.P02	Cladding replaced with blockwork around central stair core & glazing removed and blocked up.	2011.2023	RGC	LP
S4.P01	North Point added	29.04.2023	CDG	AG
no	revisions	date	source	drawn

PLANNING

PROPOSED FIRST FLOOR GA PLAN



drawn: LP checked: AG date: 05.06.2023 scale: 1:200 @ A1 carter design group

project: 8657.CDG.XX.01.DR.A.1001 S4.P02

NRP SOUTH, MULTI-STOREY CAR PARK EXTENSION

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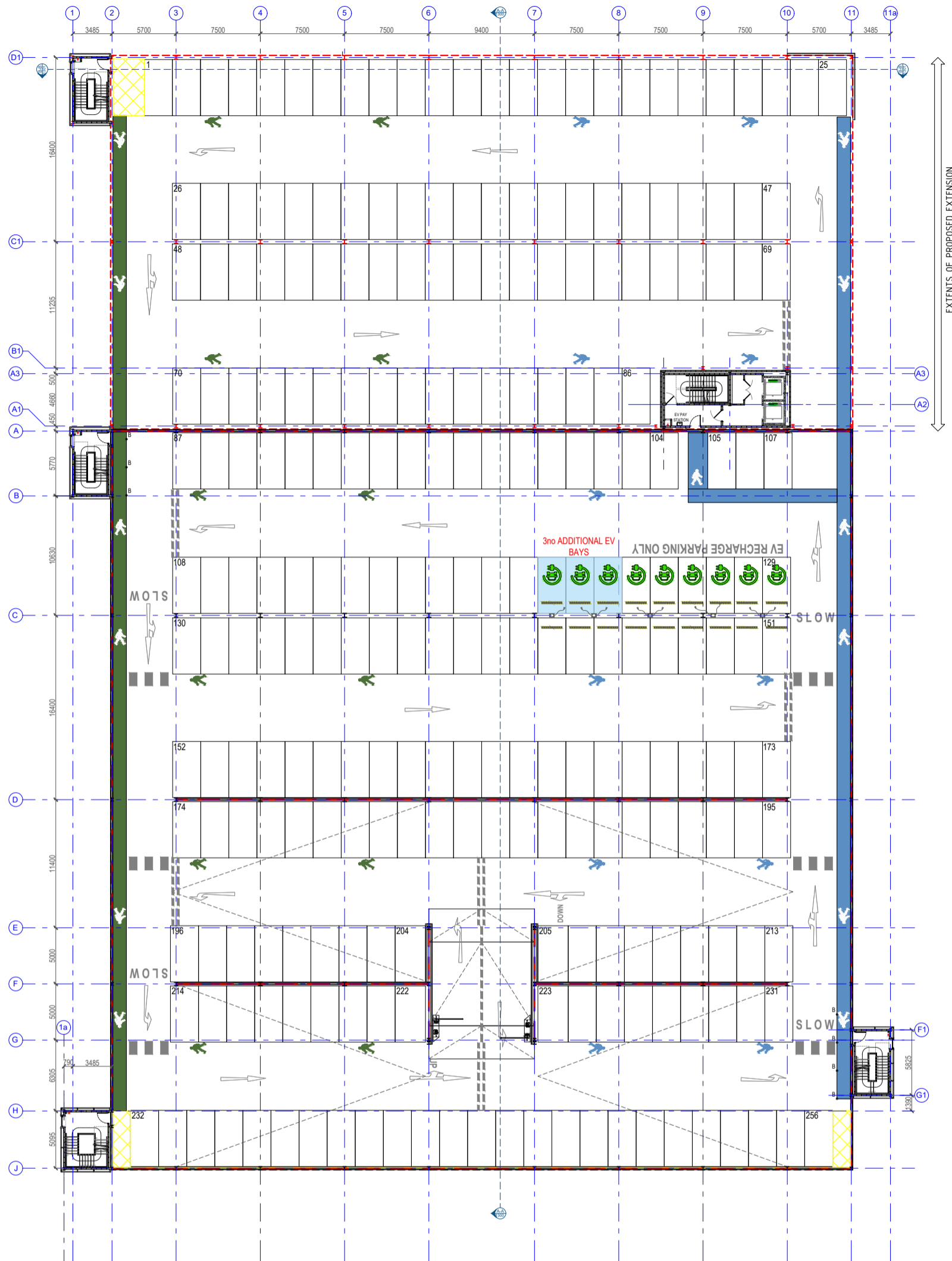
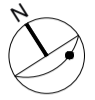
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S4.P02	Cladding replaced with blockwork around central stair core & glazing removed and blocked up.	AG	LP
S4.P01	North Point added	CDG	AG
no	revisions	date	source

PLANNING

PROJECT:
NRP SOUTH,
MULTI-STOREY CAR PARK
EXTENSION
PROPOSED SECOND FLOOR GA PLAN

cdg
Carter Design Group

drawn: LP checked: AG scale: 1:200 @ A1
date: 05.06.2023 date: 05.06.2023

PROPOSED SECOND FLOOR GA PLAN SCALE: 1:200 @ A1

8657.CDG.XX.02.DR.A.1002	S4.P02
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NRP SOUTH, MULTI-STOREY CAR PARK EXTENSION

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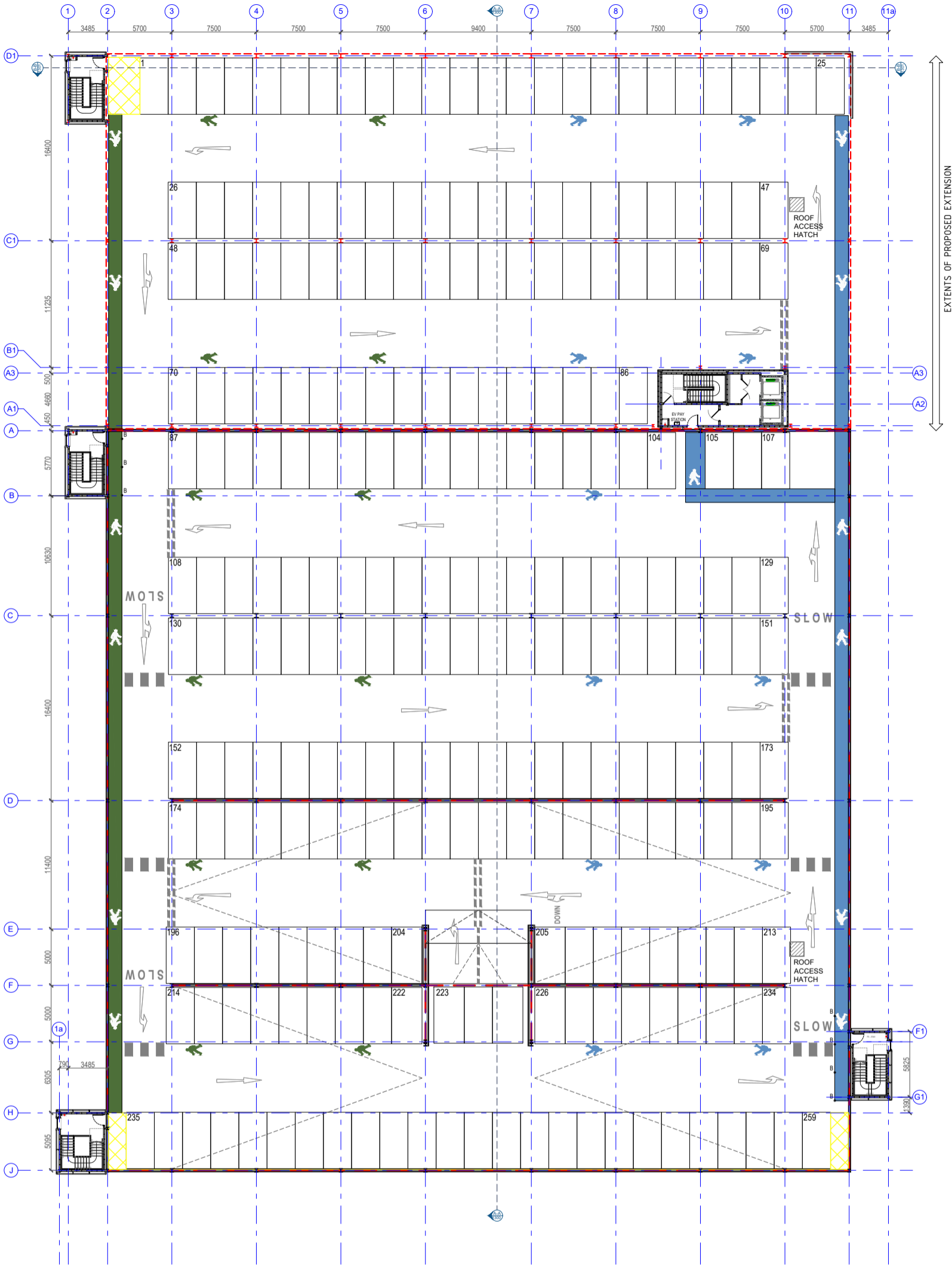
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no	revisions	date	source	drawn
S4.P03	GA Plan updated.	24.11.2023	Balfen	AG
S4.P02	Cladding replaced with blockwork around central stair core & glazing removed and blocked up.	20.11.2023	RGC	LP
S4.P01	North Point added	29.04.2023	CDG	AG

PLANNING

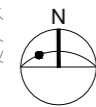
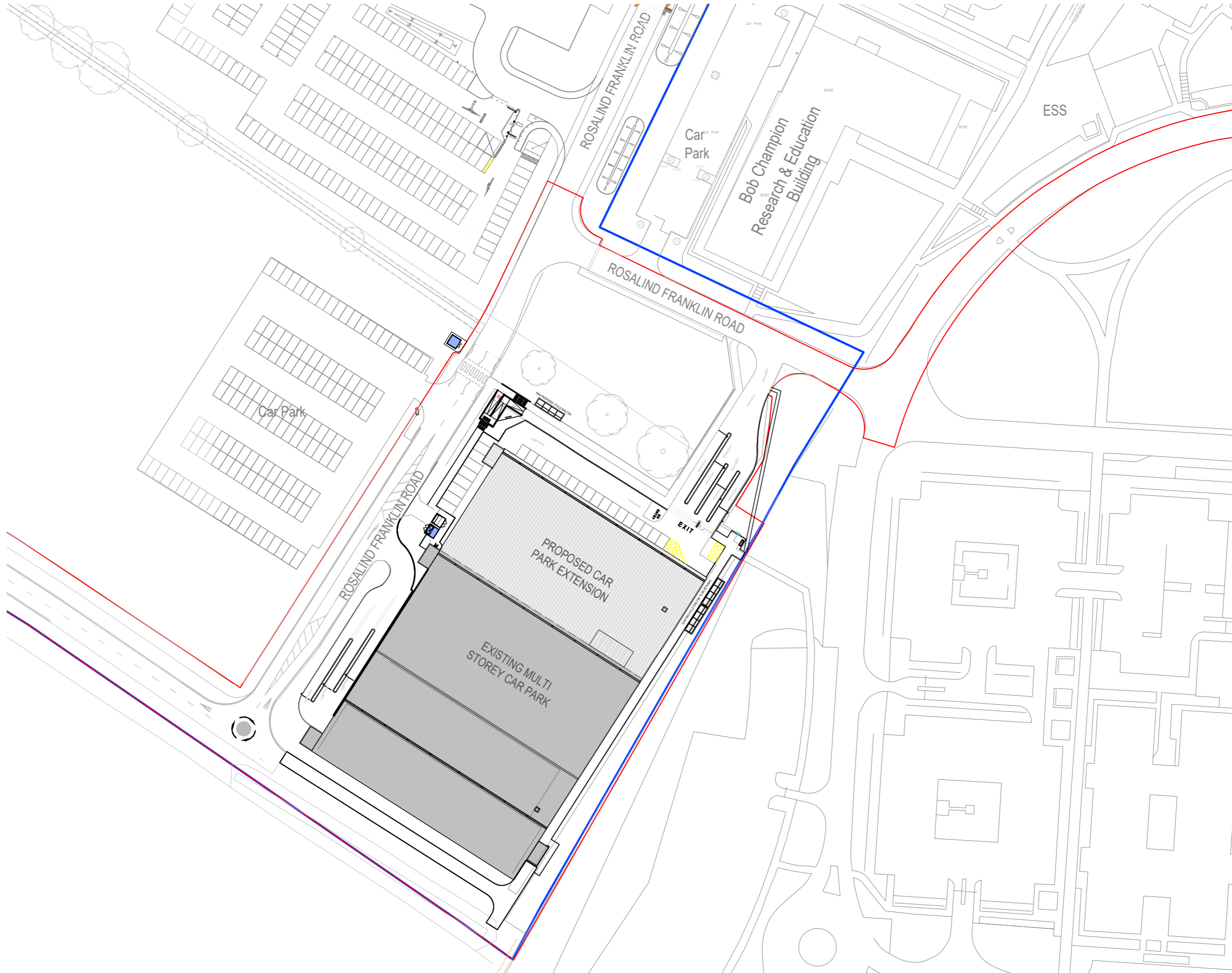
PROJECT:
NRP SOUTH,
MULTI-STOREY CAR PARK
EXTENSION
PROPOSED THIRD FLOOR GA PLAN

CDG

drawn: LP checked: AG date: 05.06.2023 scale: 1:200 @ A1

PROPOSED THIRD FLOOR GA PLAN SCALE: 1:200 @ A1

NRP SOUTH, MULTI-STOREY CAR PARK EXTENSION



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S4.P02	Additional cycle shelter included.	10.11.2023	REG	LP
S4.P01	Status code updated to 'S4'	29.04.2023	CDG	JAC
no	revisions	date	source	drawn

PLANNING

project
NRP SOUTH,
MULTI-STOREY CAR PARK
EXTENSION
drawing title
BLOCK PLAN - AS PROPOSED



drawn: LP	checked: AG	date: 05.06.2023	scale: 1:500 @ A1	Carter Design Group				
project	originator	volume	level	type	rate	number	status	revision
8657.CDG.XX.XX.DR.A.0550							S4.P02	

BLOCK PLAN - AS PROPOSED SCALE: 1:500 @ A1