

Technical Note

Proposed Residential Development 51-51a Great Underbank, Stockport

Our reference: **AM/231032/TN01**

Author: Abbie Moore

Date: 28 November 2023

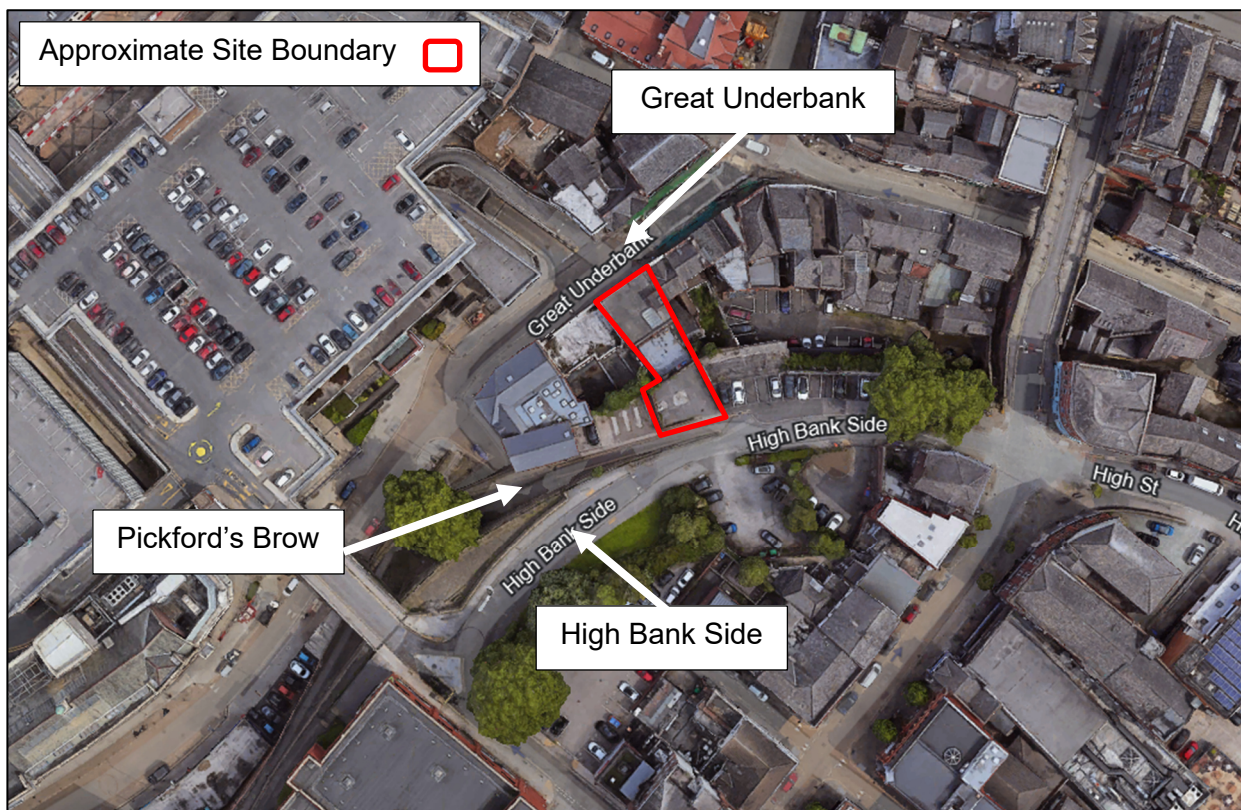
Reviewed: Peter Todd

Date: 28 November 2023

Introduction

1. SCP have been instructed by Kion Developments Ltd to provide transport planning advice in support of a planning application for a residential development on land to the south of Great Underbank and north of Pickford's Brow, Stockport.
2. Previously an application for a prior approval (Ref: DC/087456), was submitted to Stockport Metropolitan Borough Council for the Change of use of first and second floors from Class E (commercial, business and service) to Residential Use comprising 5no.apartments. Prior Approval was granted on the 3rd of February 2023 with no objection being raised from the Local Highway Authority.
3. A planning application (Ref: DC/087992) was also submitted to Stockport Metropolitan Borough Council in February 2023 for the 'Internal modifications and external works to the building, including principal facade upgrades.' This was granted full planning permission on the 29th of April 2023. This application was classed as Phase 1 of the redevelopment of the existing building.
4. This Technical Note will address Phase 2 of the proposed development, which comprises a development of 5 dwellings.
5. The site location is shown in red on **Figure 1** below:

Figure 1- Site Location



6. This Technical Note has been produced to support the planning application and provide details on the associated traffic and transport implications of the development, to inform the local highway and planning authority regarding the nature and magnitude of their impact.

Existing Conditions

7. The application site is located on land to the south of Great Underbank and north of Pickford's Bar, in Stockport town centre. The application site comprises a retail unit on the basement and ground floor with the first and second floors being vacant, off Great Underbank. The land off Pickford's Brow is situated at a higher level and is currently used as a car park. The car park is currently accessed via a simple dropped-kerb access off Pickford's Brow, as shown on **Figure 2** below. The site is predominantly surrounded by mixed-use residential and mixed-use Retail/Office.

Figure 2 – Existing Site Access



Local Highway Network

8. Great Underbank fronts the northern site boundary and provides a link between the Little Underbank/ Great Underbank junction to the north-east and Mersey Square to the south-west. Great Underbank is subject to a 20mph speed limit and benefits from footways on both sides of the carriageway. TROs in the form of double yellow lines are present on both sides of the carriageway preventing on street parking along the site's frontage. In the vicinity of the site access on Great Underbank, the traffic flow is subject to one way movement from the Little Underbank/ Great Underbank junction to the NCP car park located 30m south-west of the site.
9. Pickford's Brow is located along the site's southern boundary. Pickford Brow is a cul-de-sac off High Bank Side, with continued pedestrian access leading to Great Underbank. Pickford Brow is a single carriageway with a width of approximately 3m. Pickford Brow is subject to double yellow lines on both sides of the carriageway with no on-street parking and meets High Bank Street at a priority junction.

- 10. High Bank Street provides access to Pickford's Brow and is a one-way road offering a connection between the High Bank Side/St Petersgate/ High Street and St Peters Square junction to the northeast, with St Petersgate to the south-west. High Bank Street is a one-way road with traffic flowing from west to east from St Petersgate to the High Bank Side/St Petersgate/ High Street and St Peters Square junction. High Bank Street has a carriageway width of approximately 5m and benefits from large footpaths and street lighting on both sides of the road. TROs in the form of double yellow lines are present on both sides of the road restricting on-street parking. Several car parks are accessed from High Bank Street.

Road Safety Record

- 11. A review of accident data covering the most recently available five-year period, ending 2022, has been undertaken using Department for Transport (DfT) data. The study area analysed encompasses Great Underbank in the vicinity of the site, the Pickford's Brow/High Bank Side junction and the Little Underbank/ Great Underbank junction. The location and severity of any accidents within the study area during this period, is also presented in **Figure 3** below.

Figure 3 – 5-Year Accident Record



12. As can be seen from the above, no accidents were recorded in the study area during the 5-year study period. Overall, the recorded accident data does not demonstrate any pattern of incidents or trends that would suggest any underlying cause relating to the operation of the highway that could be affected by the development proposals.

Proposed Development

13. The development proposals consist of a residential development, comprising 5 dwellings, on land to the south of Great Underbank and north of Pickford's Brow, in Stockport.
14. The proposed site layout plan is contained in **Appendix A** and the development mix is as follows:
- 2 no. 1-bedroom apartments
 - 1 no. 2-bedroom apartments
 - 2 no. 3-bedroom townhouse

Proposed Access and Servicing Arrangement

15. The site is proposed to be a car free development. However, pedestrian access to the townhouses will be provided directly from Pickford's Brow with the apartments access being taken from Great Underbank.
16. The proposed servicing arrangement will follow that of the existing servicing arrangement associated with the previous site use, whereby refuse is collected on-street from Pickford's Brow.

Parking

17. Stockport Metropolitan Borough Council's parking standards require a maximum of 1.25 space per dwelling for sites in the Town Centre. There is a minimum requirement of 1 lockable store for cycle parking provisions.
18. The site is proposed to be a car free development. Whilst it is acknowledged that the proposed provision falls below SMBC's standards, this is considered acceptable in this instance for the following reasons:-

- i. As detailed later, the site benefits from high levels of accessibility, being within easy access of Stockport Town centre (less than 200m walk) and associated facilities, amenities and numerous transport links, including bus stops and Stockport Railway Station (or <700m walk distance). Therefore, prospective residents will not be wholly reliant on the private car to travel to/from the site;
- ii. SMBC's parking standards are given as a maximum.
- iii. The previously approved bike store on the ground floor of the apartment aspect will provide one space per proposed apartment. Additionally, each town house will contain under-stair cupboards which can accommodate a bicycle. Therefore, the development is in compliance with the minimum requirements for bicycles and therefore provides a large benefit to the car free development.
- iv. The previous planning application (Ref: DC/087456) for prior approval of 5 dwellings was approved in February 2023, which proposed no parking and was given no highway objection received.
- v. The general thrust of National and Local planning policy is also to reduce car borne trips and encourage travel by sustainable modes such as public transport, walking and cycling. The proposed development takes full advantage of this highly accessible location and, by providing a level of parking below the Council's standards, will help to reduce the reliance on the use of the private car and meet these policy objectives.
- vi. Any prospective purchaser or tenant of the apartments will be in no doubt as to the level of parking provided at the scheme and will therefore decide whether to take up occupancy accordingly;
- vii. The key junctions and critical sections of highway in the vicinity of the site, are protected by parking restrictions, which helps to ensure that parking does not result in any road safety or operational issues.
- viii. Additionally, there are a high number of 'pay and display' parking spaces available within the immediate vicinity of the site (which accommodate for short and long stay parking), along with large pay and display and multi-story car parks, namely NCP Mersey Shopping Centre Car Park, Churchgate Carpark, Piccadilly Car Park etc.

19. The car free development is therefore considered to be acceptable in this highly accessible location, particularly given that car ownership levels in this area are very low.

Accessibility

Pedestrian Accessibility

20. The MfS states that walkable neighbourhoods are typically characterised by having a range of facilities within 10 minutes' (up to about 800m) walking distance of residential areas which residents may access comfortably on foot. However, it goes on to state that this is not an upper limit and that walking offers the greatest potential to replace short car trips, particularly those under 2km.
21. The site is within an acceptable walk distance of Stockport town centre and the vast array of amenities the town of Stockport has on offer including retail, education and healthcare. The site is also within an acceptable walk distance of numerous transport facilities to encourage prospective residents to travel via sustainable modes. The closest bus stop is located on St Peter's Square, approximately 190m (>3 min walk) south-west of the site, and Stockport Railway Station is located to the south-west of the site and can be accessed in under a 11-minute walk time (or <700m walk distance).
22. The local area benefits from natural surveillance from the businesses and houses that abut all the main walking routes. The local area is well lit and generally benefits from wide footways.

Cycle Accessibility

23. The nearby areas of Cheadle, and Bredbury, amongst others, are all located within the 5km catchment area from the development site. The topography of the area is generally conducive to cycling, so the site is therefore well located to encourage prospective residents to travel via bicycle.
24. There are several National Cycle Routes (NCRs) surrounding the site. NCR 62 is located to the north of the site and provides a connection between NCR's 55 and 558 in the north-west, with Hadfield to the north-east.

Public Transport

25. In terms of bus services, the Chartered Institute of Highways & Transportation's (CIHT's) "*Guidelines for Planning for Public Transport in Developments*" document identifies, at section 6.20, that "*Bus stops are located to minimise passengers' walking distance to their final*

destination. The maximum walking distance to a bus stop should not exceed 400m and preferably be no more than 300m.”

26. As detailed earlier, the closest bus stop is located on St Peter’s Square, approximately 190m (>3 min walks) south-west of the site, although there are other bus stops within the recommended walk distance that provide additional services. The frequency and services using this bus stop are detailed in **Table 1** below.

Table 1: Bus Services

Bus Service	Route	Frequency		
		Mon-Fri	Sat	Sun
309	Stockport - Cheadle Heath Circular	Approx ever 60 mins	Approx ever 60 mins	Approx ever 120 mins
310	Stockport - Cheadle Heath Circular	Approx ever 60 mins	Approx ever 60 mins	Approx ever 120 mins
312	Stockport Merseyway Precinct - Stockport Merseyway Precinct	Approx ever 30 mins	Approx ever 30 mins	Approx ever 30 mins
314	Stockport - Offerton Circular	Approx ever 20 mins	Approx ever 20 mins	Approx ever 30 mins
358	Stockport - Hayfield	Approx ever 60 mins	Approx ever 60 mins	Approx ever 60 mins
364	Stockport - Woodbank Park/Heaton Norris circular	Approx ever 60 mins	Approx ever 60 mins	-
375	Stockport - Hawk Green	2 services a day	2 services a day	6 services a day
383	Stockport - Romiley Circular	Approx ever 15 mins	Approx ever 15 mins	Approx ever 30 mins
385	Mellor-Stockport	Approx ever 60 mins	Approx ever 60 mins	-

27. In terms of rail services, Stockport Railway Station is located to the south-west of the site and can be accessed in under a 11-minute walk time (or <700m walk distance) and is therefore well within an acceptable walking and cycling distance. The railway station provides direct services throughout the week including services to Norwich, Liverpool Lime Street, Stoke-on-Trent, Alderley Edge, Manchester Piccadilly and London Euston, amongst others.

[Accessibility Summary](#)

28. Having regard to the above, it is considered that the site benefits from high levels of accessibility by sustainable modes and has a large range of local amenities within close proximity. Access to the site on foot and by cycle is of a good standard and there are multiple transport facilities within close proximity providing access to a range of local destinations. These findings demonstrate that prospective residents will not be wholly reliant on the private car.

Summary and Conclusions

29. Based on the information provided within this technical note, we consider the proposed 5 no. residential dwellings, to be acceptable with regard to transport and highway matters.
30. The Technical Note has illustrated and described that the site can be accessed at a good level by all modes.
31. With regard to the local highway network, the estimated level of traffic demand generated by the proposed 5 no. dwellings, would result in a negligible impact, which therefore cannot be considered severe in the context of the NPPF.
32. The personal injury accident data for the most recently available 5-year period demonstrates that the area in vicinity of the site does not have any recurring highway safety problems that could be affected by the development proposals.
33. The proposed development is car free. However, this is considered acceptable given the highly sustainable location of the site.
34. It is therefore concluded that the proposed development is acceptable with regard to transport.

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APPENDIX A



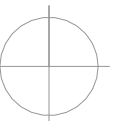
1 PLN - Proposed Site Plan

scale: 1 : 200



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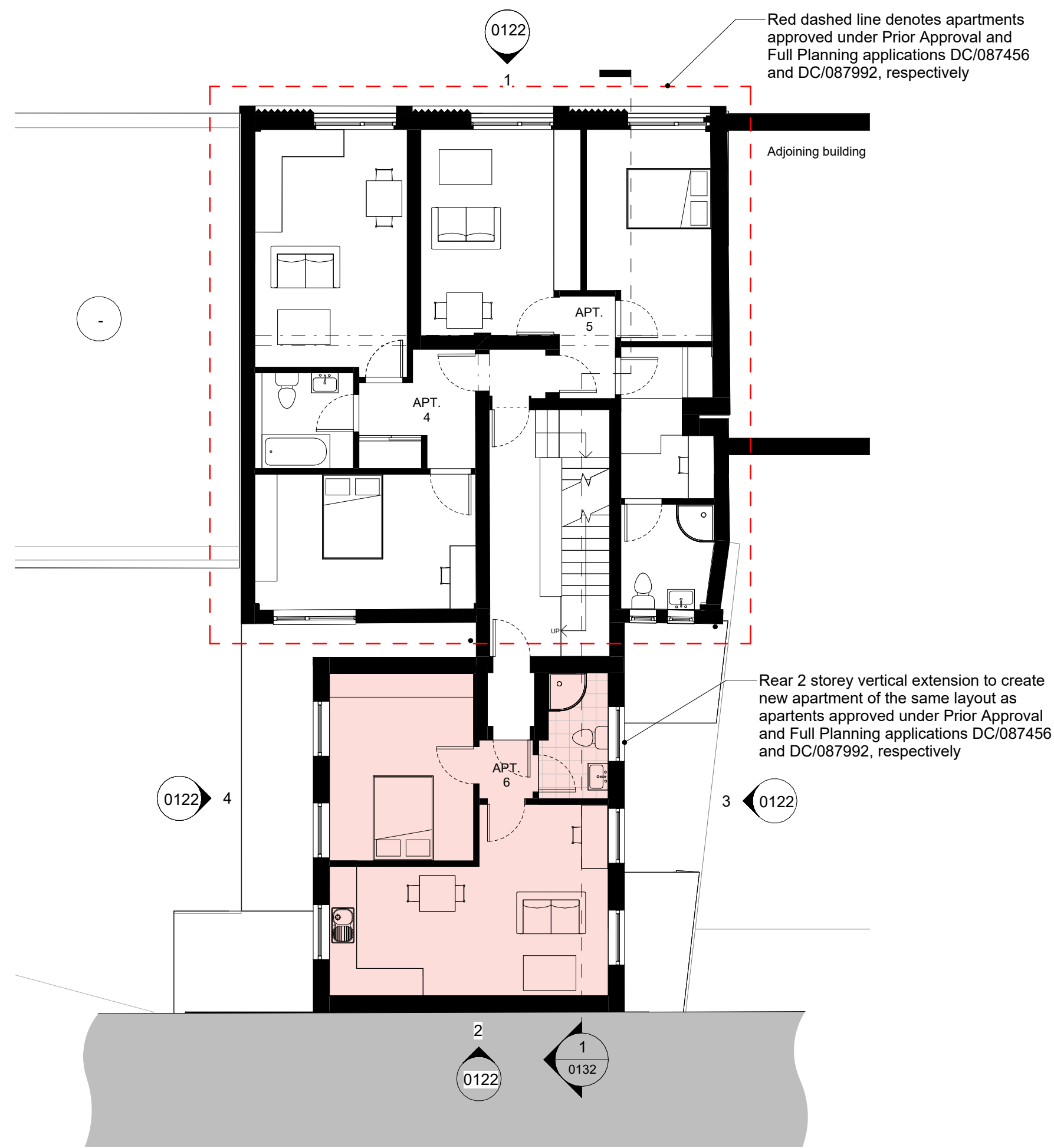
job title
51 Great Underbank

drawing title
**GA Site
Proposed Site Plan**

client name	project address	status	scale
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		drawn SAA	checked PCK
			current issue 17.01.24

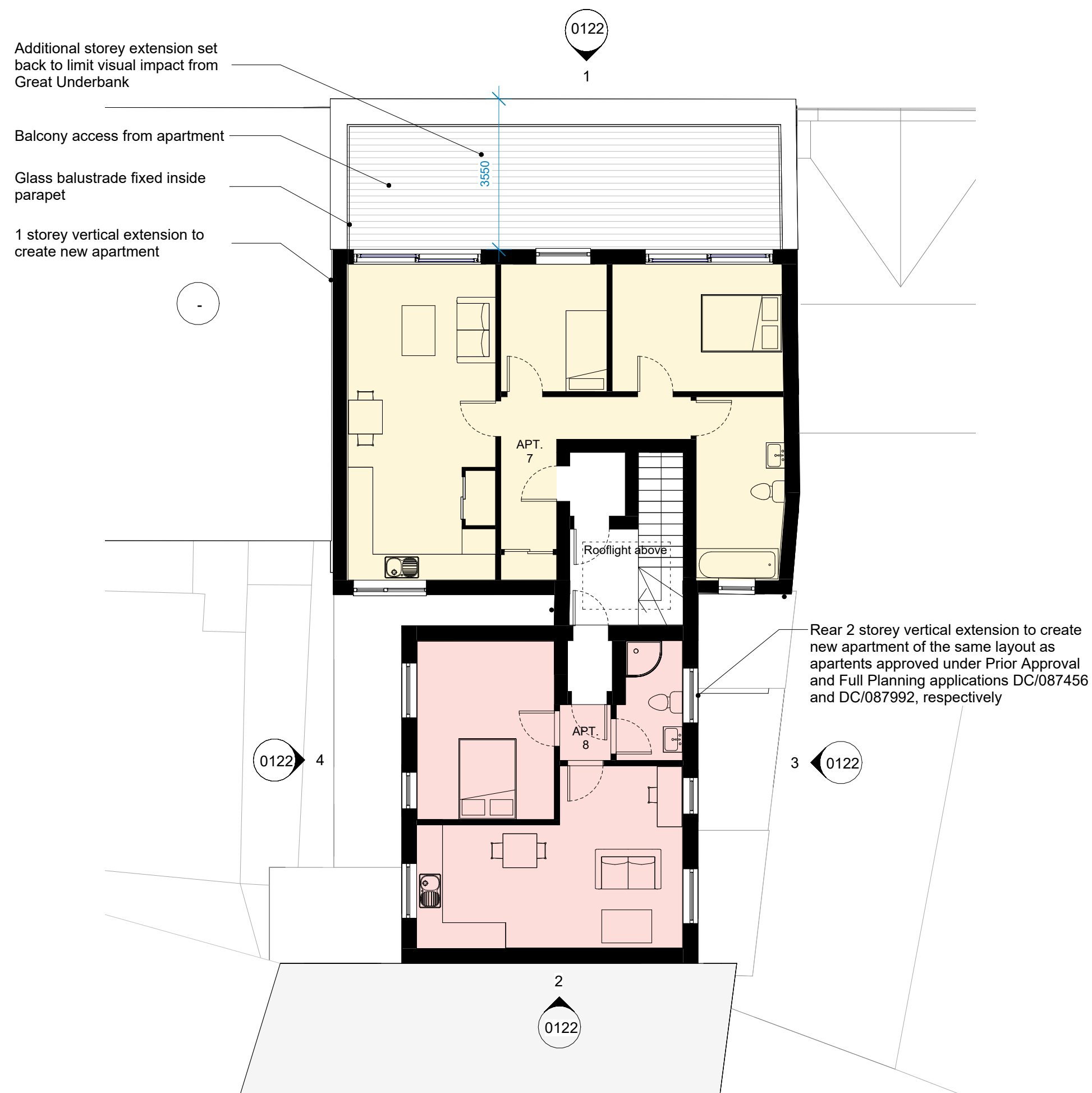
drawing no. **0091-KA-XX-ZZ-DR-A-0101** revision **1**

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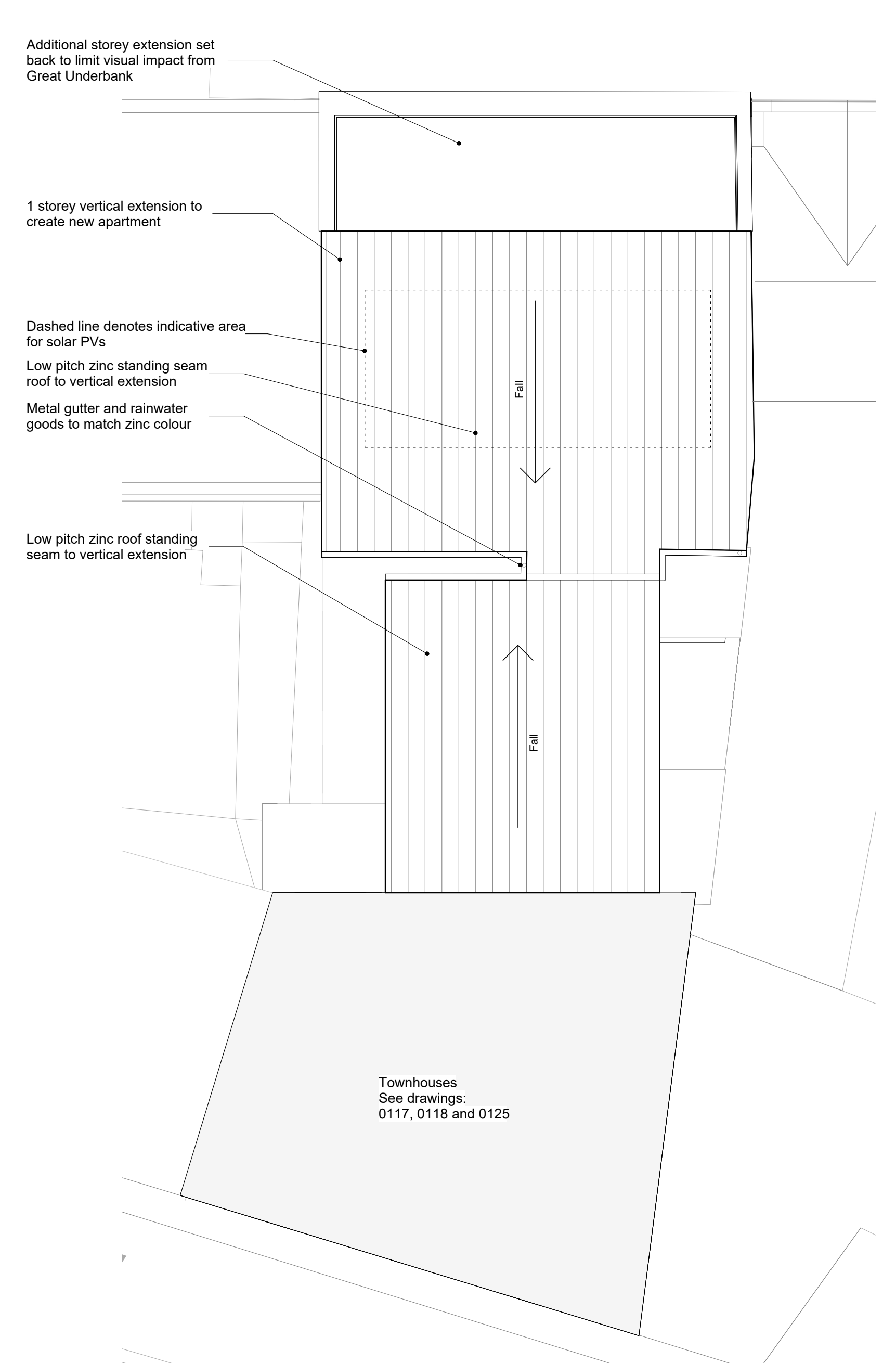
1 PLN - Proposed Apartments Level 02 (Great Underbank)

scale: 1 : 100



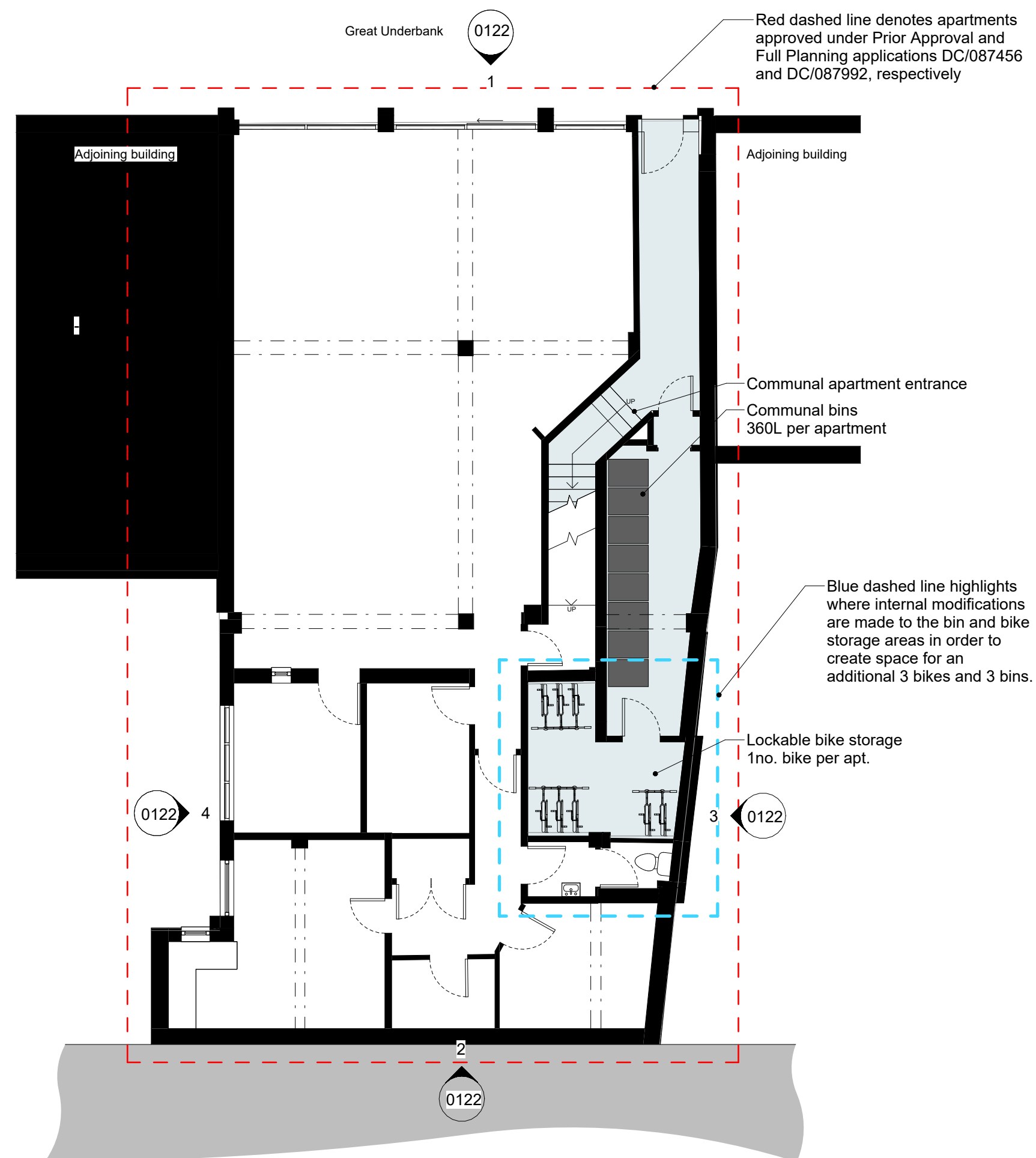
2 PLN - Proposed Apartments Level 03 (Great Underbank)

scale: 1 : 100



3 PLN - Apartments Roof Plan (Great Underbank)

scale: 1 : 100



4 PLN - Proposed Apartments Level 00 (Great Underbank)

scale: 1 : 100

Townhouses
See drawings:
0117, 0118 and 0125

Townhouses
See drawings:
0117, 0118 and 0125



51 Great Underbank

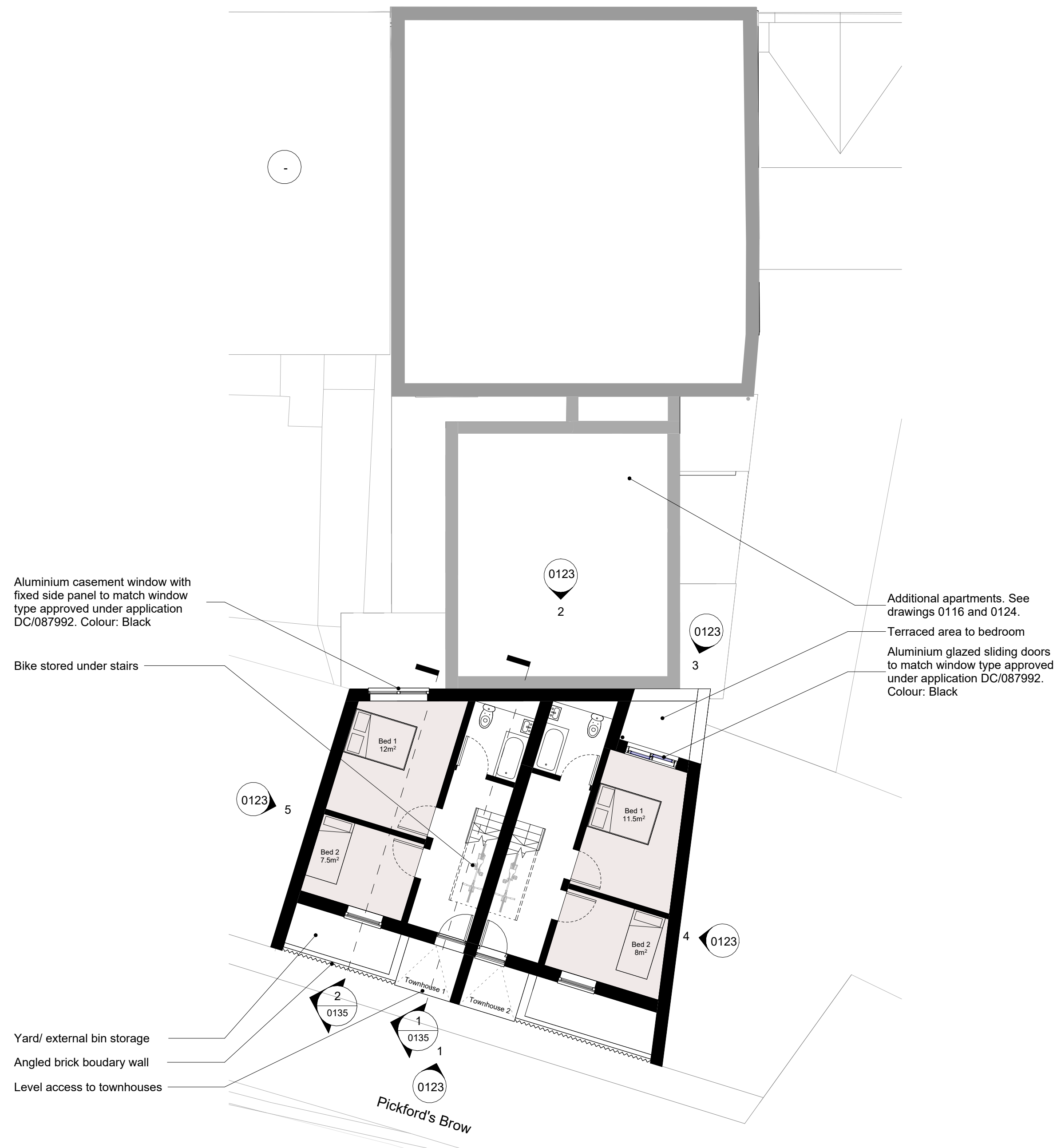
GA Plan
Proposed Floor & Roof Plans - Great Underbank

Client Name	Project Name	Project Status	Scale
Kion Developments	51/51A Great Underbank Stockport	PLANNING SK1 1NE	1:100 @ A1
Drawn By	SA	Checked By	PCK
Date	13.02.24		

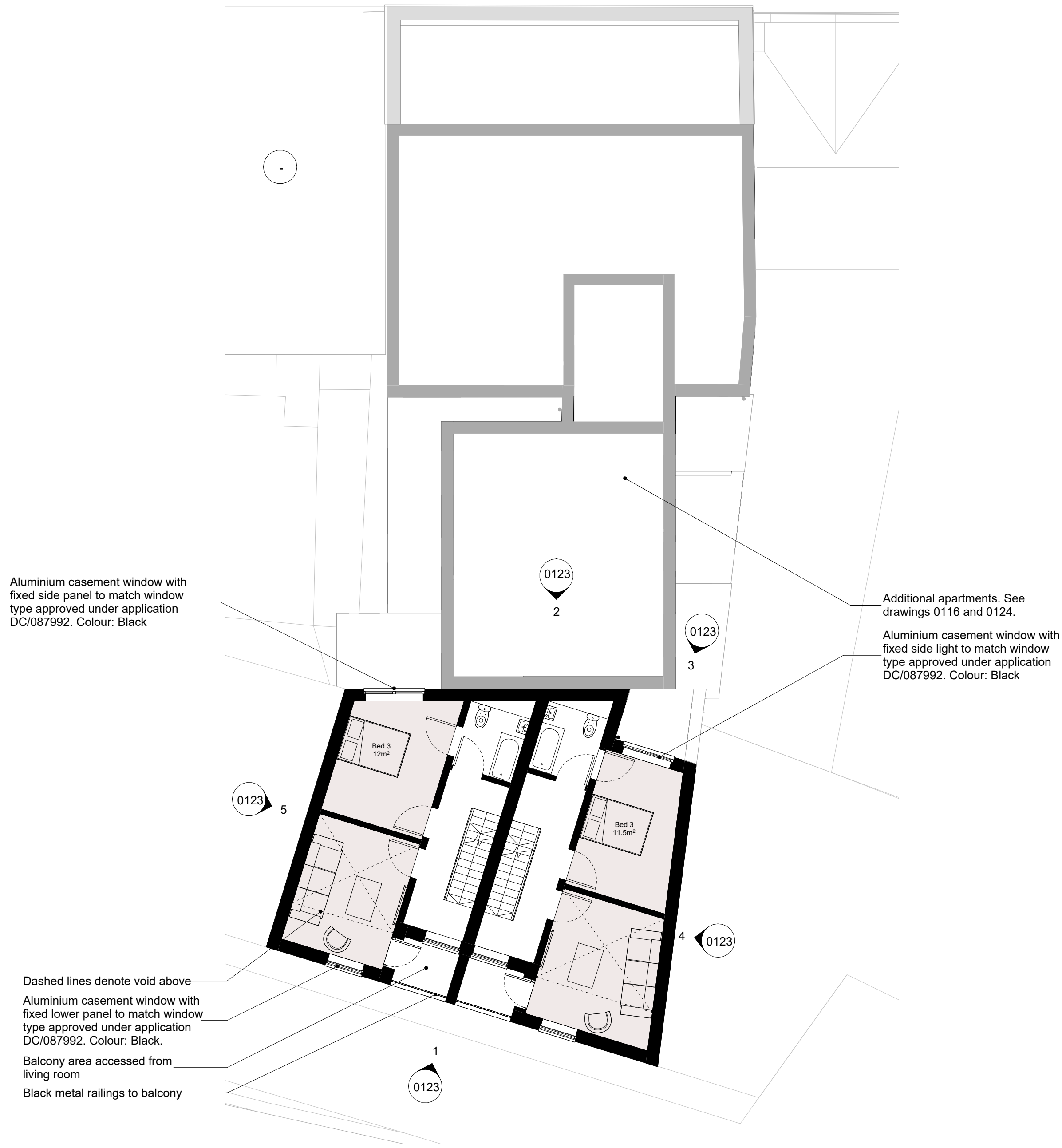
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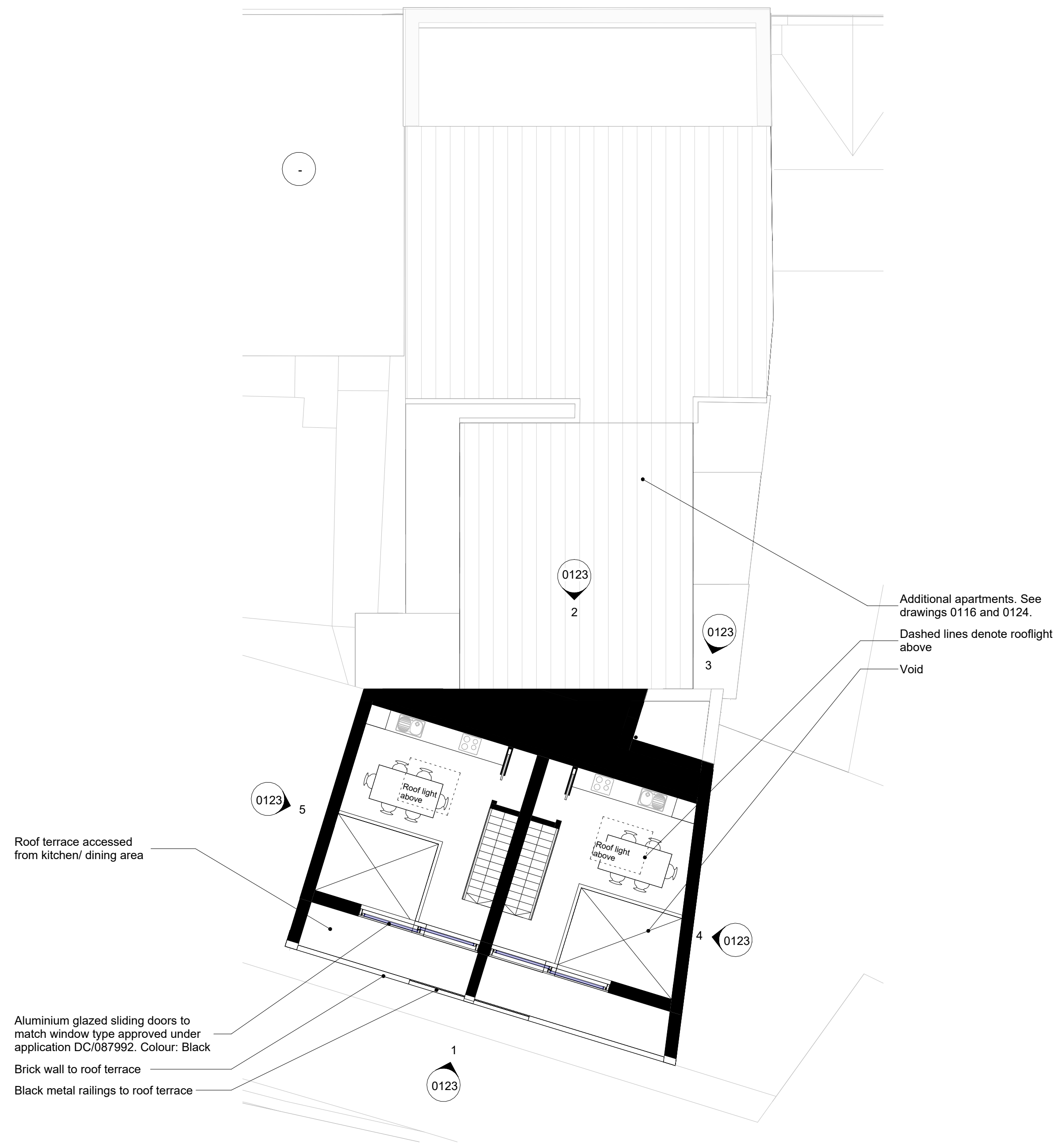
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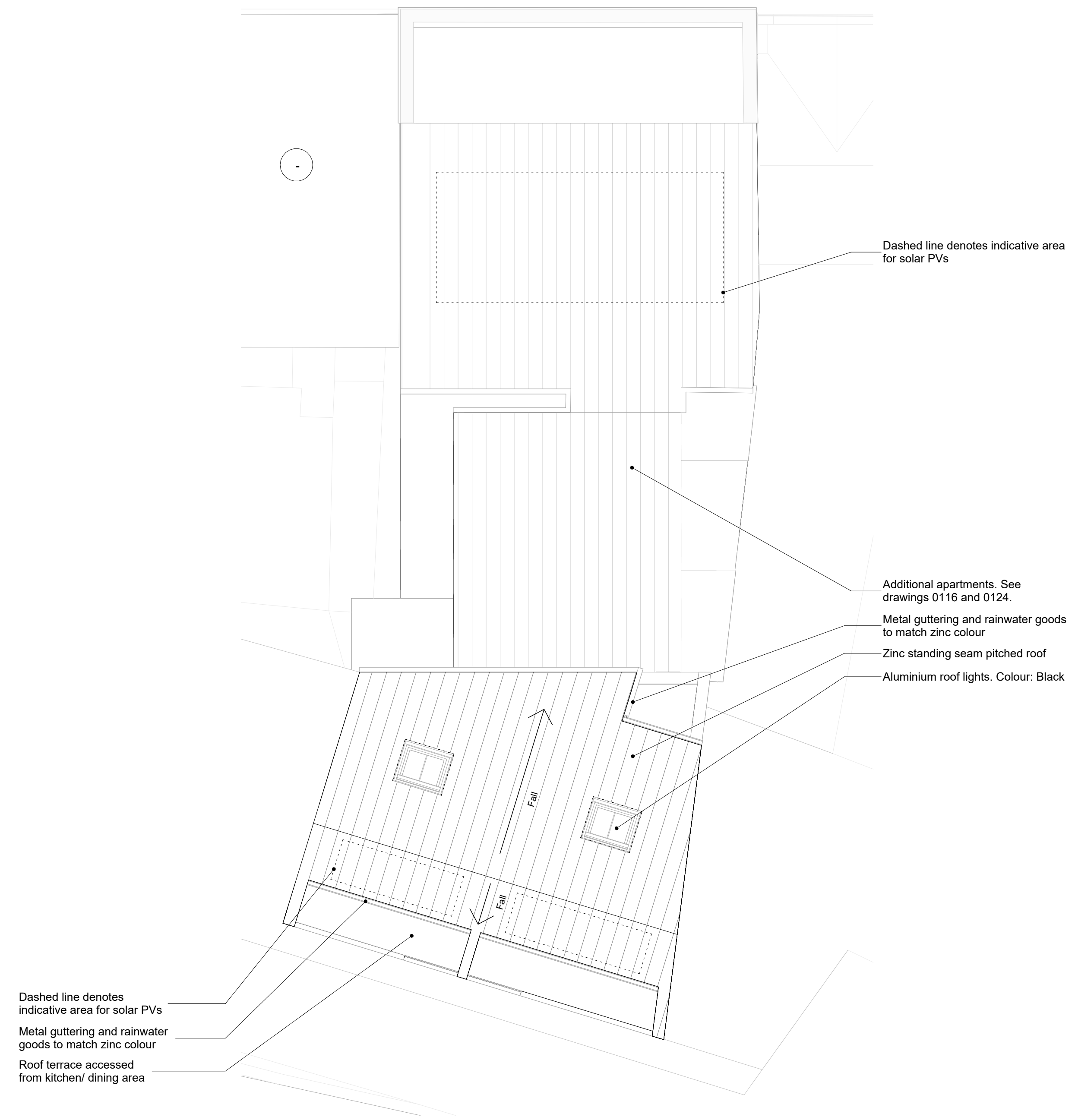
1 PLN - Proposed Townhouses Level 0 (Pickford's Brow)
scale: 1 : 100



2 PLN - Proposed Townhouses Level 01 (Pickford's Brow)
scale: 1 : 100



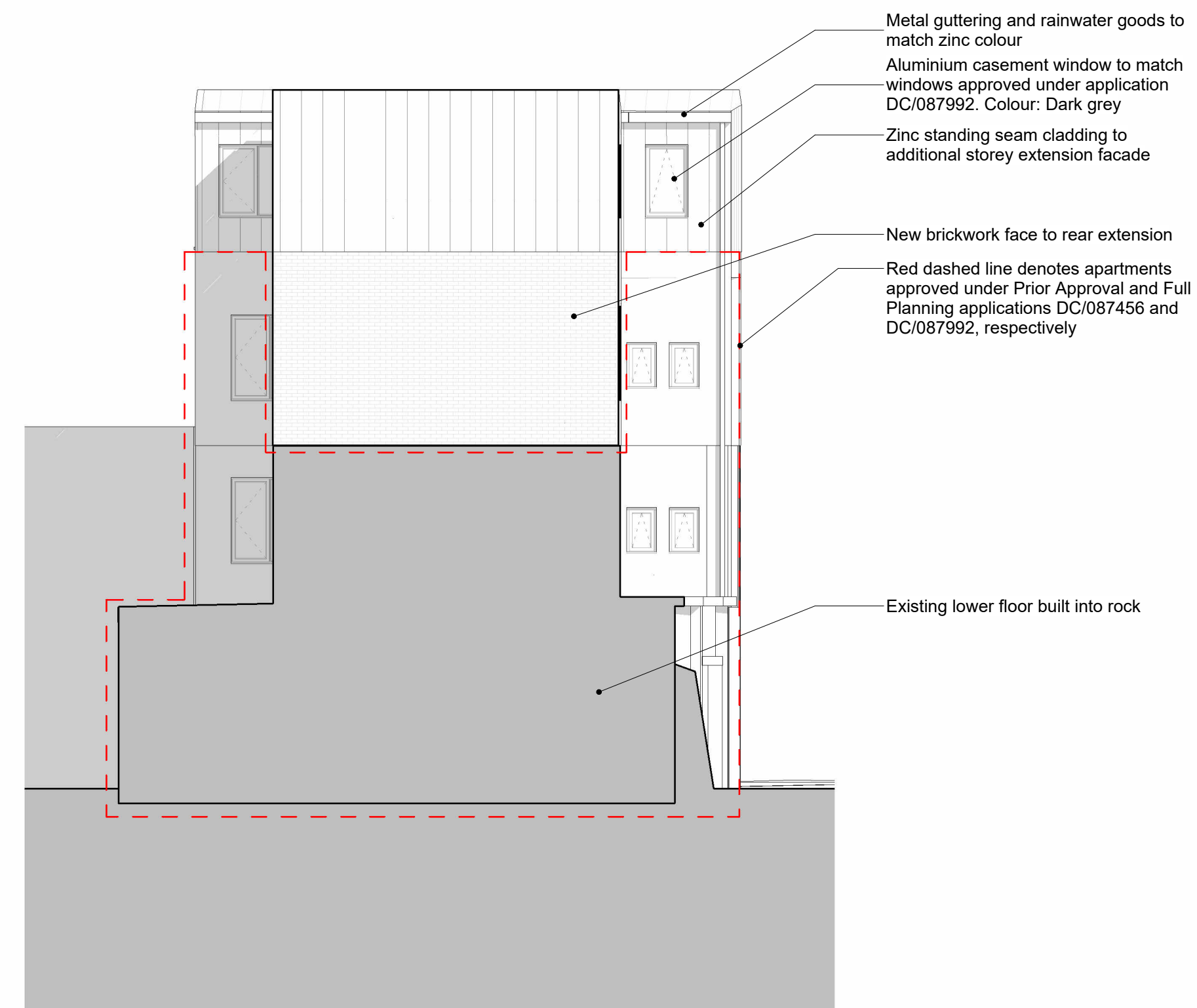
1 PLN - Proposed Townhouses Level 02 Plan (Pickford's Brow)
scale: 1 : 100



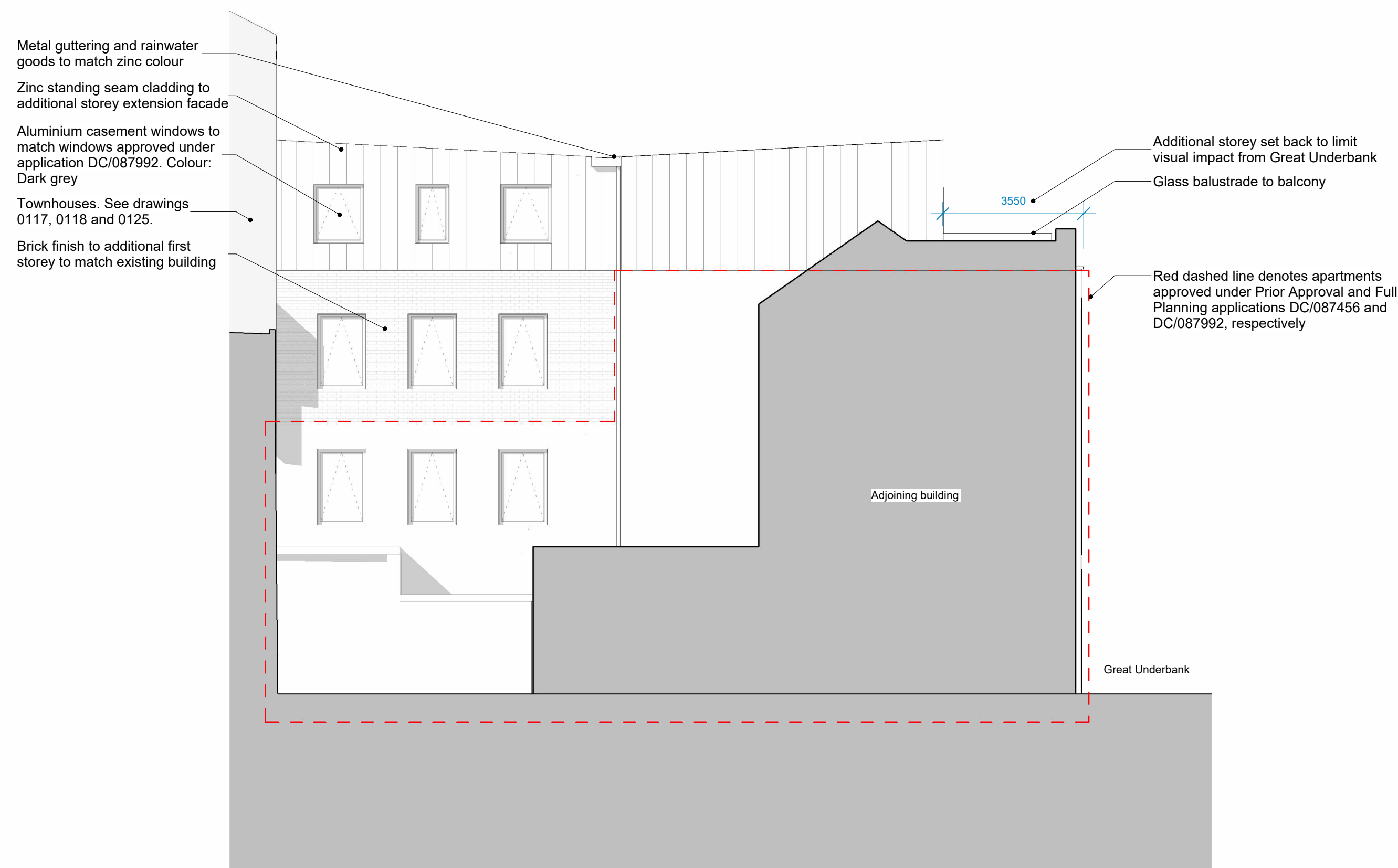
2 PLN - Proposed Townhouses Roof Plan (Pickford's Brow)
scale: 1 : 100



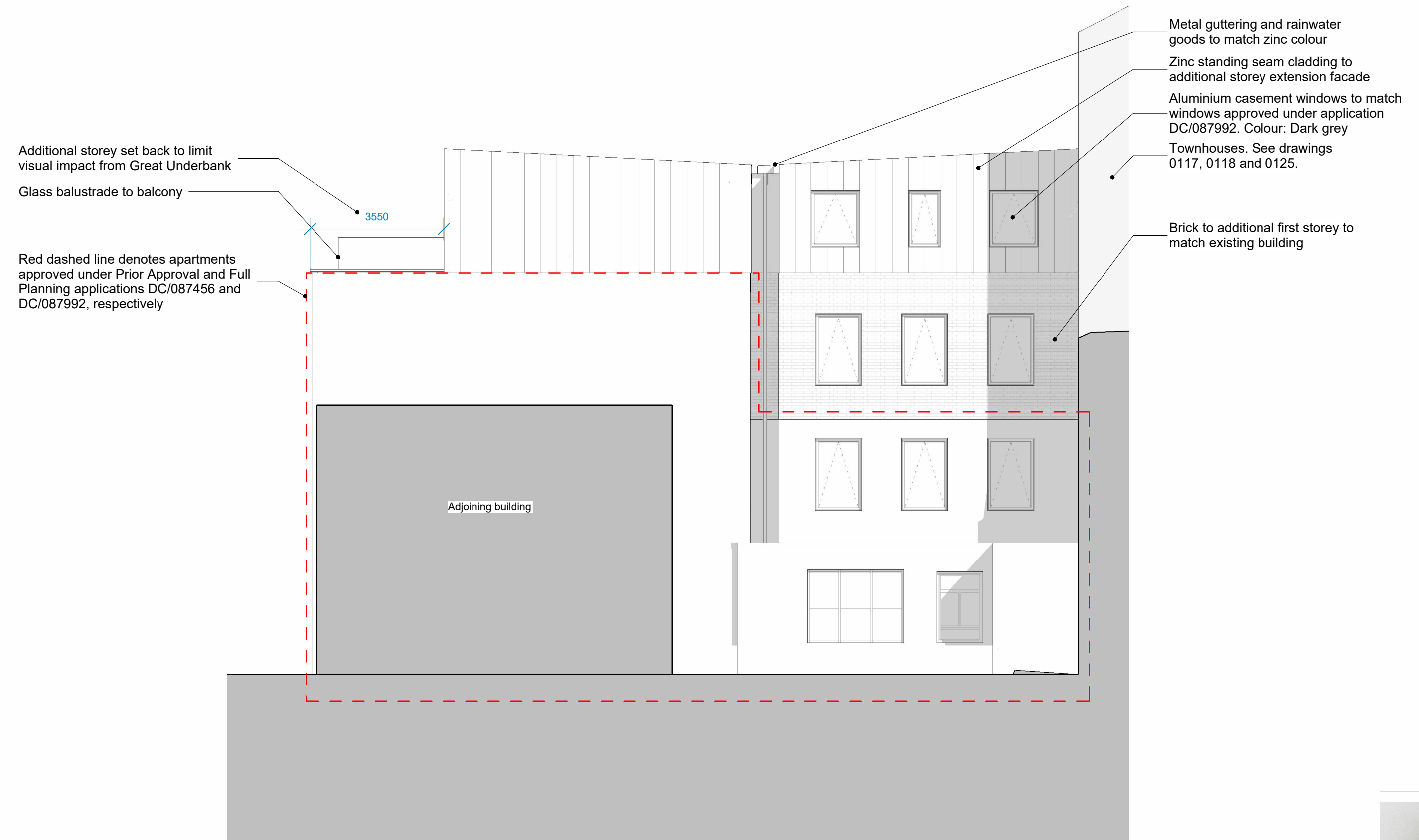
1 ELV - Proposed Apartments North West Elevation (Great Underbank)
scale: 1 : 100



2 ELV - Proposed Apartments South East Elevation (Great Underbank)
scale: 1 : 100



3 ELV - Proposed Apartments North East Elevation (Great Underbank)
scale: 1 : 100



4 ELV - Proposed Apartments South West Elevation (Great Underbank)
scale: 1 : 100



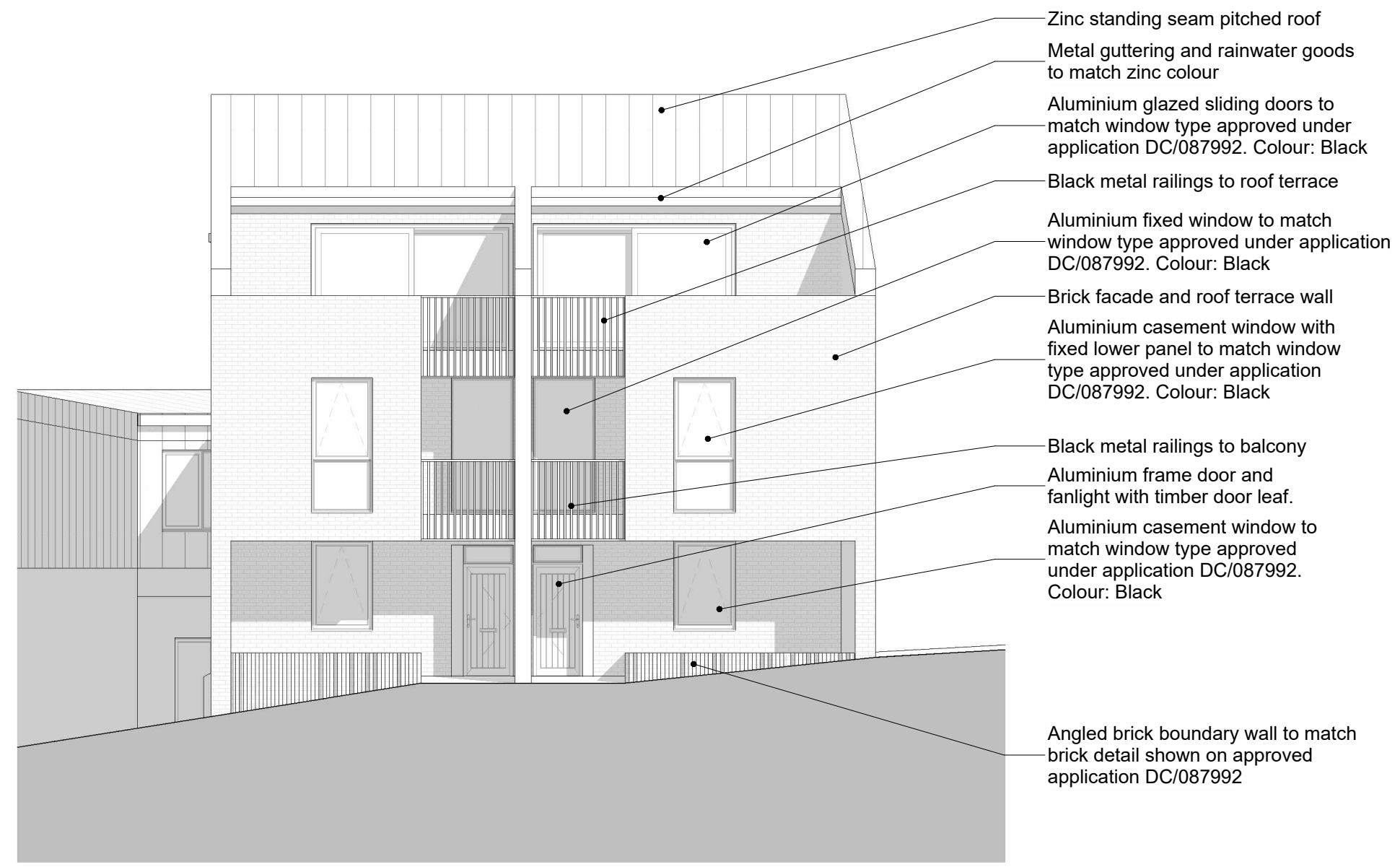
51 Great Underbank

GA Elevation
Proposed Elevations - Great Underbank

Client Name	Project Name	Project Status	Scale
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	SK1 1NE	SAAs PCK	17.01.24

0091-KA-XX-ZZ-DR-A-0122

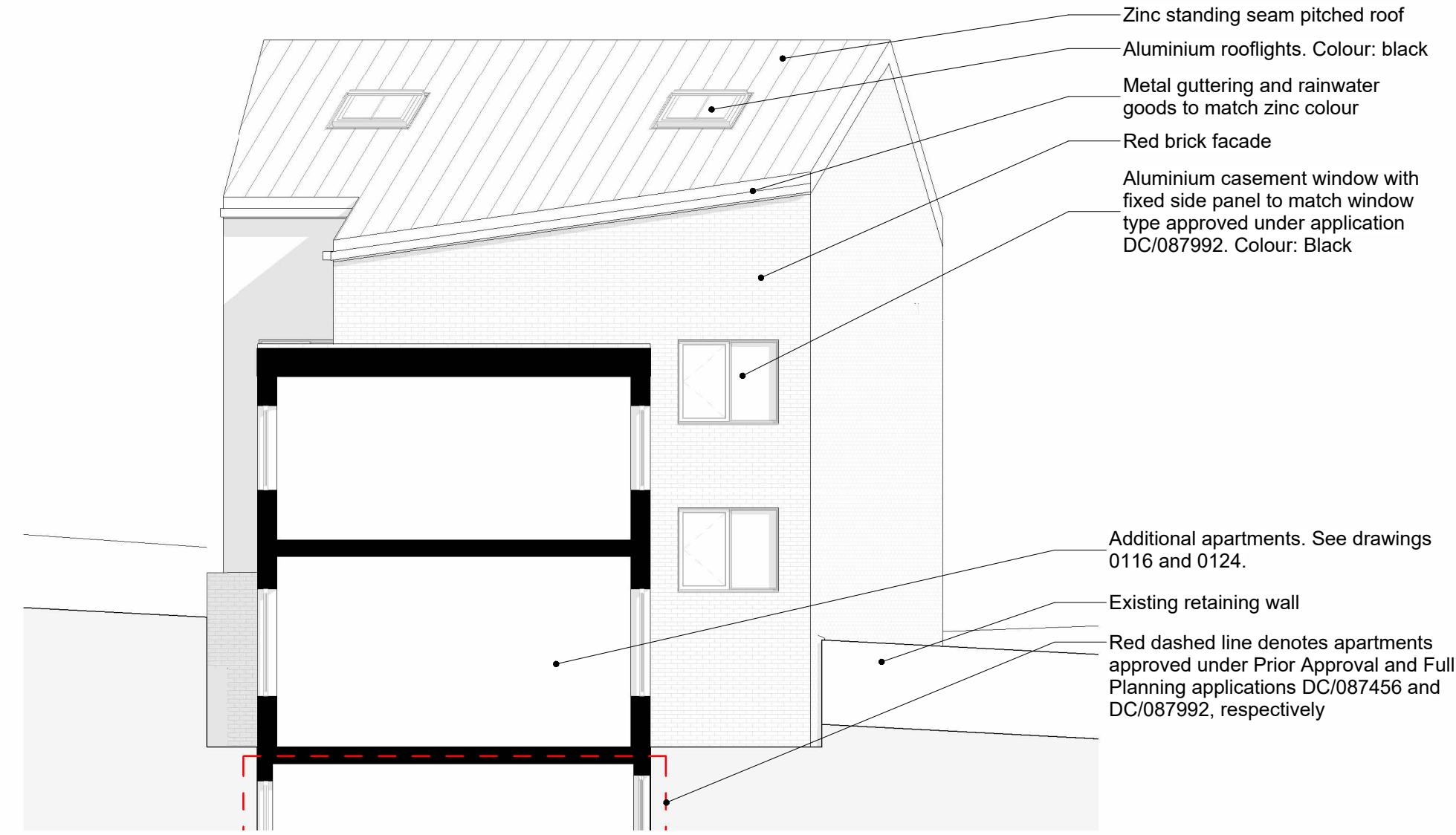
2



- Zinc standing seam pitched roof
- Metal guttering and rainwater goods to match zinc colour
- Aluminium glazed sliding doors to match window type approved under application DC/087992. Colour: Black
- Black metal railings to roof terrace
- Aluminium fixed window to match window type approved under application DC/087992. Colour: Black
- Brick facade and roof terrace wall
- Aluminium casement window with fixed lower panel to match window type approved under application DC/087992. Colour: Black
- Black metal railings to balcony
- Aluminium frame door and fanlight with timber door leaf.
- Aluminium casement window to match window type approved under application DC/087992. Colour: Black
- Angled brick boundary wall to match brick detail shown on approved application DC/087992

1 ELV - Proposed Townhouses South East Elevation (Pickford's Brow)

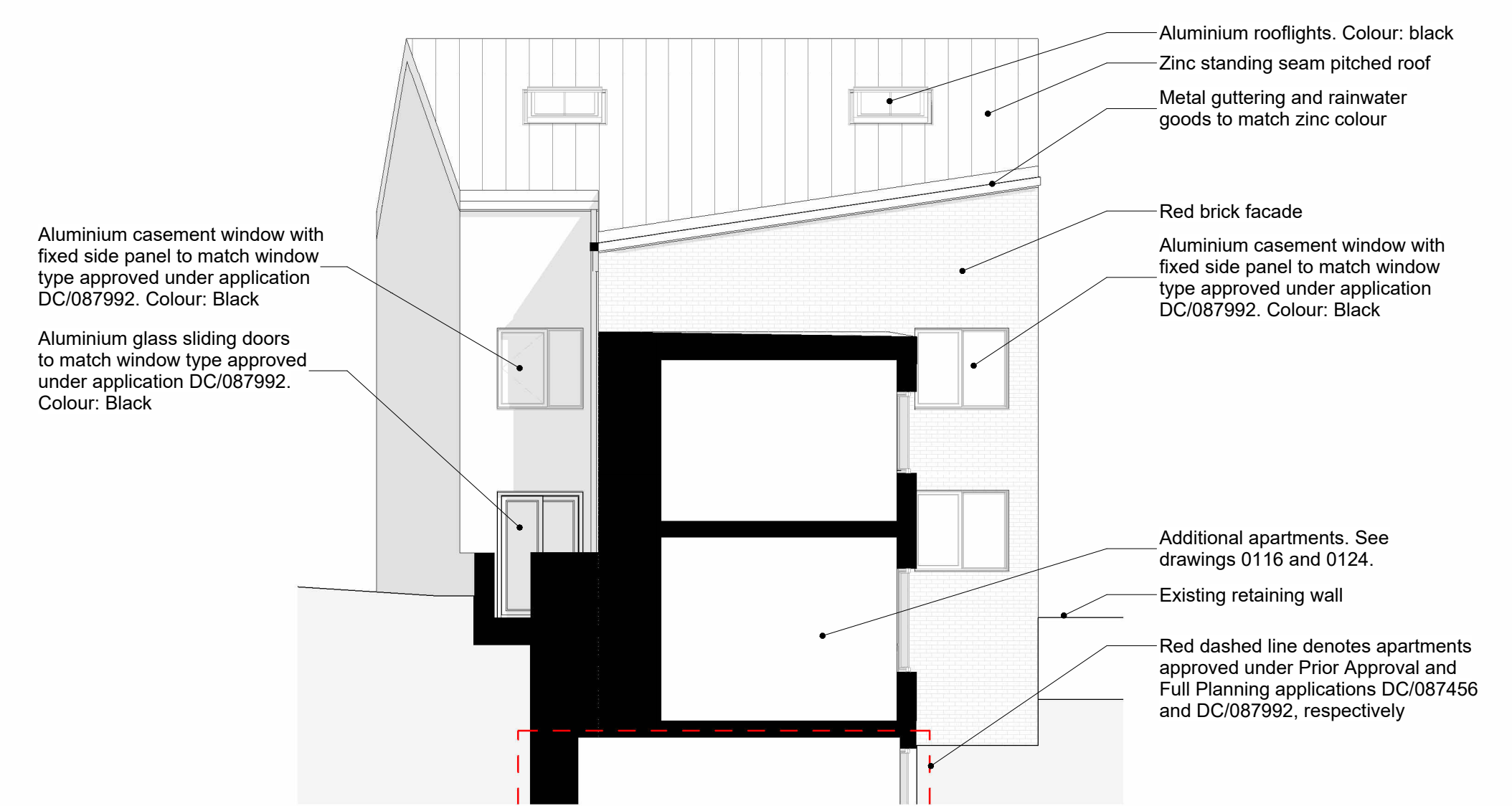
scale: 1 : 100



- Zinc standing seam pitched roof
- Aluminium rooflights. Colour: black
- Metal guttering and rainwater goods to match zinc colour
- Red brick facade
- Aluminium casement window with fixed side panel to match window type approved under application DC/087992. Colour: Black
- Additional apartments. See drawings 0116 and 0124.
- Existing retaining wall
- Red dashed line denotes apartments approved under Prior Approval and Full Planning applications DC/087456 and DC/087992, respectively

2 ELV - Proposed Townhouses North West Elevation (Pickford's Brow)

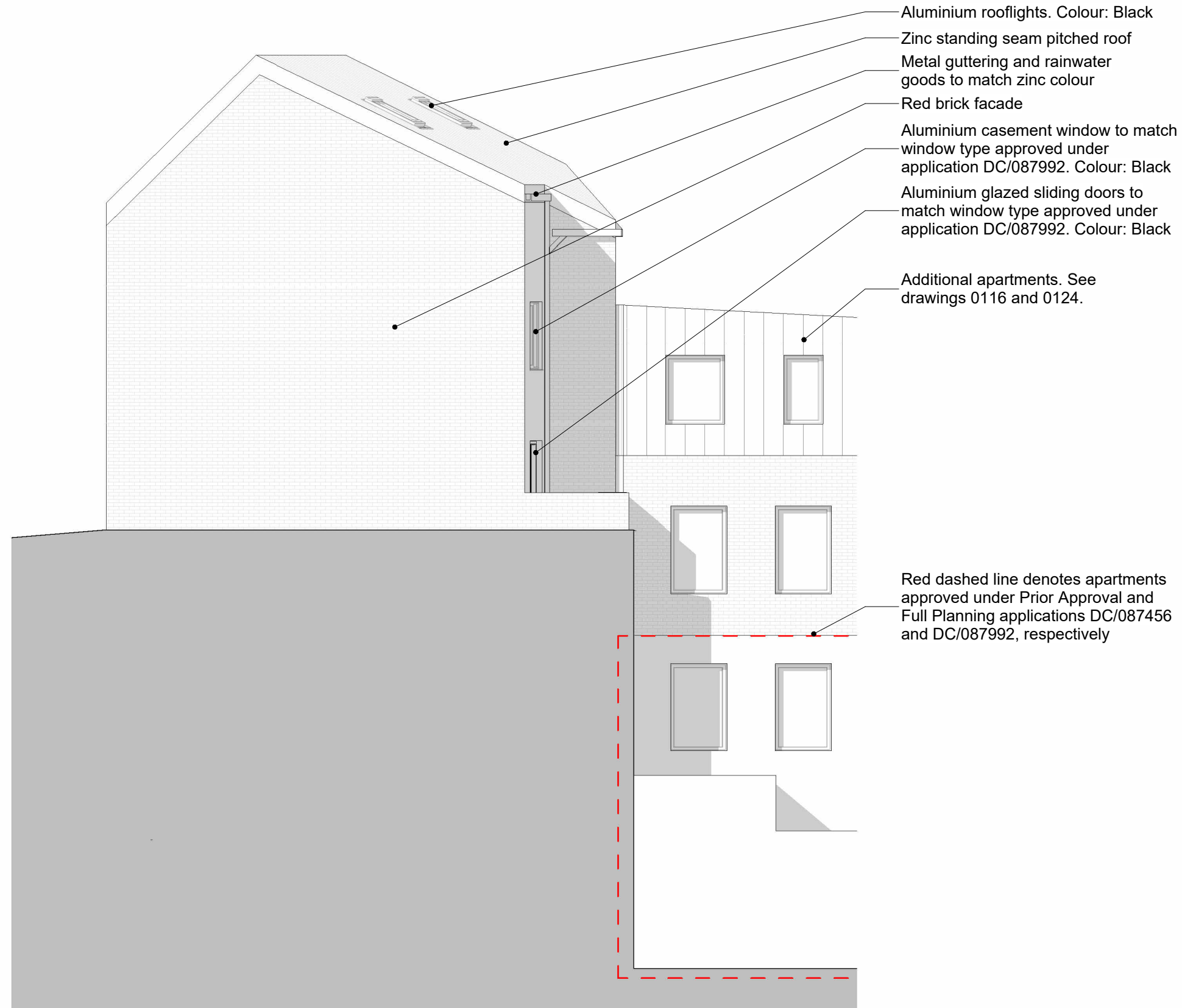
scale: 1 : 100



- Aluminium rooflights. Colour: black
- Zinc standing seam pitched roof
- Metal guttering and rainwater goods to match zinc colour
- Red brick facade
- Aluminium casement window with fixed side panel to match window type approved under application DC/087992. Colour: Black
- Aluminium glass sliding doors to match window type approved under application DC/087992. Colour: Black
- Additional apartments. See drawings 0116 and 0124.
- Existing retaining wall
- Red dashed line denotes apartments approved under Prior Approval and Full Planning applications DC/087456 and DC/087992, respectively

3 ELV - Proposed Townhouse North Elevation (Pickford's Brow)

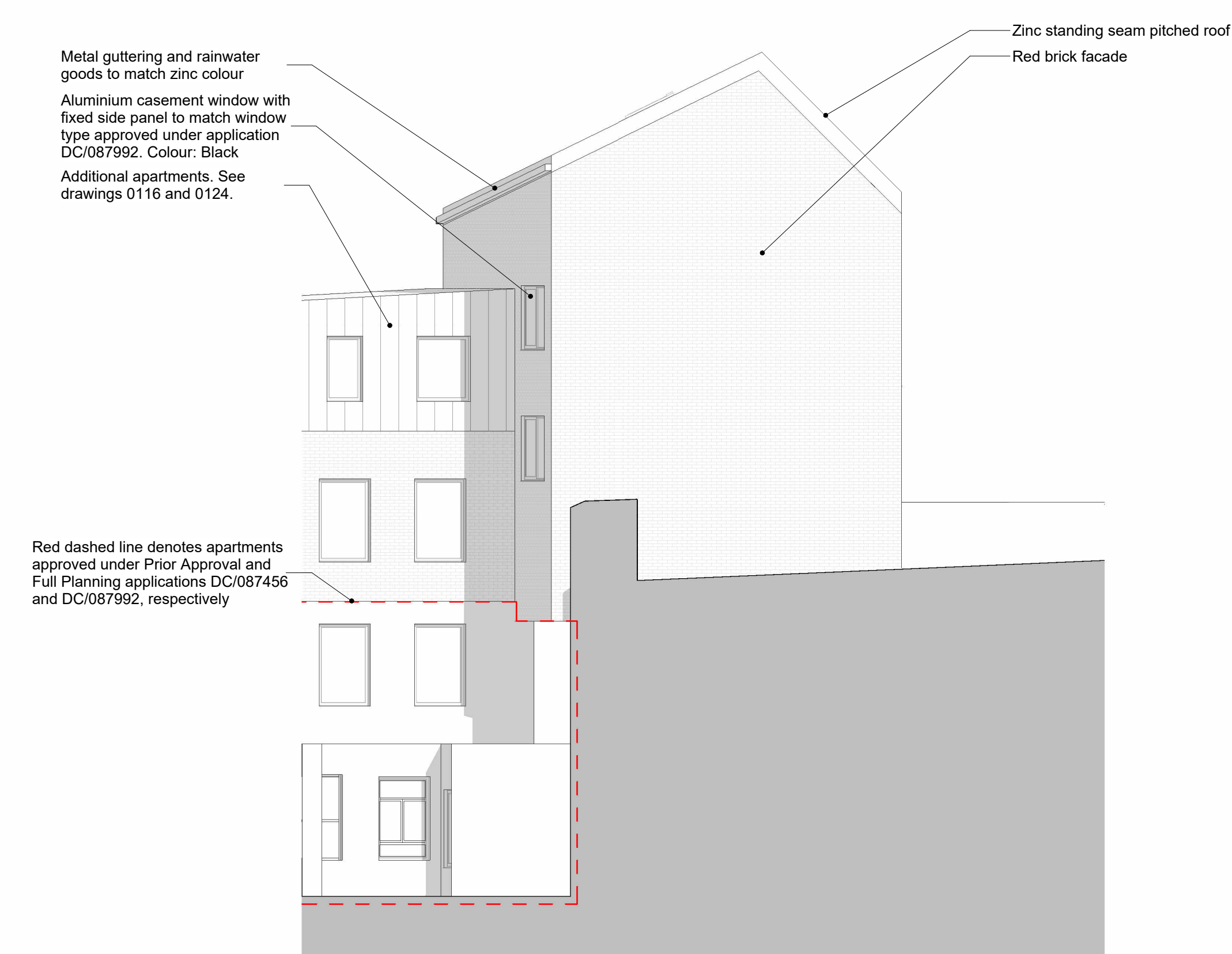
scale: 1 : 100



- Aluminium rooflights. Colour: Black
- Zinc standing seam pitched roof
- Metal guttering and rainwater goods to match zinc colour
- Red brick facade
- Aluminium casement window to match window type approved under application DC/087992. Colour: Black
- Aluminium glazed sliding doors to match window type approved under application DC/087992. Colour: Black
- Additional apartments. See drawings 0116 and 0124.
- Red dashed line denotes apartments approved under Prior Approval and Full Planning applications DC/087456 and DC/087992, respectively

4 ELV - Proposed Townhouses North East Elevation (Pickford's Brow)

scale: 1 : 100



- Metal guttering and rainwater goods to match zinc colour
- Aluminium casement window with fixed side panel to match window type approved under application DC/087992. Colour: Black
- Additional apartments. See drawings 0116 and 0124.
- Red dashed line denotes apartments approved under Prior Approval and Full Planning applications DC/087456 and DC/087992, respectively
- Zinc standing seam pitched roof
- Red brick facade

5 ELV - Proposed Townhouses South West Elevation (Pickford's Brow)

scale: 1 : 100



51 Great Underbank

GA Elevation
Proposed Townhouse Elevations - Pickford's Brow

Client Name	Project Address	Project Name	Scale
Kion Developments	51/51A Great Underbank Skipton SK1 1NE	PLANNING	1:100 @ A1
		SAA PCK	17.01.24

0091-KA-XX-ZZ-DR-A-0123

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APPENDIX B

TRIP RATE CALCULATION SELECTION PARAMETERS:

Land Use : 03 - RESIDENTIAL
 Category : C - FLATS PRIVATELY OWNED
 MULTI-MODAL TOTAL VEHICLES

Selected regions and areas:

02	SOUTH EAST	
	CT CENTRAL BEDFORDSHIRE	3 days
	HF HERTFORDSHIRE	1 days
	PO PORTSMOUTH	1 days
	SC SURREY	1 days
	SS SOUTHEND ON SEA	2 days
03	SOUTH WEST	
	DC DORSET	1 days
04	EAST ANGLIA	
	NF NORFOLK	1 days
	SF SUFFOLK	2 days
06	WEST MIDLANDS	
	WM WEST MIDLANDS	1 days
07	YORKSHIRE & NORTH LINCOLNSHIRE	
	KS KIRKLEES	1 days
	LS LEEDS	1 days
08	NORTH WEST	
	AC CHESHIRE WEST & CHESTER	1 days
	GM GREATER MANCHESTER	2 days
	MS MERSEYSIDE	2 days
09	NORTH	
	CU CUMBERLAND	1 days
10	WALES	
	CO CONWY	1 days
	FS FLINTSHIRE	1 days
11	SCOTLAND	
	HI HIGHLAND	1 days
	SA SOUTH AYRSHIRE	1 days
	SR STIRLING	2 days

This section displays the number of survey days per TRICS® sub-region in the selected set

SCP York Street Manchester

Licence No: 726001

Primary Filtering selection:

This data displays the chosen trip rate parameter and its selected range. Only sites that fall within the parameter range are included in the trip rate calculation.

Parameter: No of Dwellings
Actual Range: 6 to 175 (units:)
Range Selected by User: 6 to 184 (units:)

Parking Spaces Range: All Surveys Included

Parking Spaces per Dwelling Range: All Surveys Included

Bedrooms per Dwelling Range: All Surveys Included

Percentage of dwellings privately owned: All Surveys Included

Public Transport Provision:

Selection by: Include all surveys

Date Range: 01/01/00 to 11/05/22

This data displays the range of survey dates selected. Only surveys that were conducted within this date range are included in the trip rate calculation.

Selected survey days:

Monday	4 days
Tuesday	8 days
Wednesday	5 days
Thursday	7 days
Friday	3 days

This data displays the number of selected surveys by day of the week.

Selected survey types:

Manual count	27 days
Directional ATC Count	0 days

This data displays the number of manual classified surveys and the number of unclassified ATC surveys, the total adding up to the overall number of surveys in the selected set. Manual surveys are undertaken using staff, whilst ATC surveys are undertaken using machines.

Selected Locations:

Town Centre	4
Edge of Town Centre	23

This data displays the number of surveys per main location category within the selected set. The main location categories consist of Free Standing, Edge of Town, Suburban Area, Neighbourhood Centre, Edge of Town Centre, Town Centre and Not Known.

Selected Location Sub Categories:

Development Zone	3
Residential Zone	11
Built-Up Zone	10
No Sub Category	3

This data displays the number of surveys per location sub-category within the selected set. The location sub-categories consist of Commercial Zone, Industrial Zone, Development Zone, Residential Zone, Retail Zone, Built-Up Zone, Village, Out of Town, High Street and No Sub Category.

Inclusion of Servicing Vehicles Counts:

Servicing vehicles Included	7 days - Selected
Servicing vehicles Excluded	20 days - Selected

Secondary Filtering selection:

Use Class:

C3 27 days

This data displays the number of surveys per Use Class classification within the selected set. The Use Classes Order (England) 2020 has been used for this purpose, which can be found within the Library module of TRICS@.

Population within 500m Range:

All Surveys Included

Secondary Filtering selection (Cont.):

Population within 1 mile:

10,001 to 15,000	9 days
15,001 to 20,000	3 days
20,001 to 25,000	3 days
25,001 to 50,000	12 days

This data displays the number of selected surveys within stated 1-mile radii of population.

Population within 5 miles:

50,001 to 75,000	8 days
75,001 to 100,000	4 days
125,001 to 250,000	7 days
250,001 to 500,000	5 days
500,001 or More	3 days

This data displays the number of selected surveys within stated 5-mile radii of population.

Car ownership within 5 miles:

0.6 to 1.0	9 days
1.1 to 1.5	17 days
1.6 to 2.0	1 days

This data displays the number of selected surveys within stated ranges of average cars owned per residential dwelling, within a radius of 5-miles of selected survey sites.

Travel Plan:

Yes	2 days
No	25 days

This data displays the number of surveys within the selected set that were undertaken at sites with Travel Plans in place, and the number of surveys that were undertaken at sites without Travel Plans.

PTAL Rating:

No PTAL Present	27 days
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This data displays the number of selected surveys with PTAL Ratings.

Covid-19 Restrictions	Yes	At least one survey within the selected data set was undertaken at a time of Covid-19 restrictions
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LIST OF SITES relevant to selection parameters

1	AC-03-C-01 NEW CRANE STREET CHESTER	BLOCKS OF FLATS		CHESHIRE WEST & CHESTER
	Edge of Town Centre Residential Zone Total No of Dwellings:		60	
	<i>Survey date: FRIDAY</i>		<i>17/10/08</i>	<i>Survey Type: MANUAL</i>
2	CO-03-C-01 MOSTYN BROADWAY LLANDUDNO	BLOCKS OF FLATS		CONWY
	Edge of Town Centre Built-Up Zone Total No of Dwellings:		37	
	<i>Survey date: MONDAY</i>		<i>26/03/18</i>	<i>Survey Type: MANUAL</i>
3	CT-03-C-01 WING ROAD LEIGHTON BUZZARD LINSLADE	BLOCKS OF FLATS		CENTRAL BEDFORDSHIRE
	Edge of Town Centre Residential Zone Total No of Dwellings:		175	
	<i>Survey date: TUESDAY</i>		<i>15/05/18</i>	<i>Survey Type: MANUAL</i>
4	CT-03-C-02 STANBRIDGE ROAD LEIGHTON BUZZARD	BLOCKS OF FLATS		CENTRAL BEDFORDSHIRE
	Edge of Town Centre Residential Zone Total No of Dwellings:		62	
	<i>Survey date: TUESDAY</i>		<i>15/05/18</i>	<i>Survey Type: MANUAL</i>
5	CT-03-C-03 COURT DRIVE DUNSTABLE	BLOCKS OF FLATS		CENTRAL BEDFORDSHIRE
	Edge of Town Centre No Sub Category Total No of Dwellings:		146	
	<i>Survey date: TUESDAY</i>		<i>15/05/18</i>	<i>Survey Type: MANUAL</i>
6	CU-03-C-01 KING STREET CARLISLE	BLOCK OF FLATS		CUMBERLAND
	Town Centre Built-Up Zone Total No of Dwellings:		40	
	<i>Survey date: THURSDAY</i>		<i>12/06/14</i>	<i>Survey Type: MANUAL</i>
7	DC-03-C-01 ABBOTSBURY ROAD WEYMOUTH	BLOCKS OF FLATS		DORSET
	Edge of Town Centre Residential Zone Total No of Dwellings:		27	
	<i>Survey date: TUESDAY</i>		<i>08/07/08</i>	<i>Survey Type: MANUAL</i>
8	FS-03-C-01 WREXHAM STREET MOLD	BLOCK OF FLATS		FLINTSHIRE
	Edge of Town Centre Built-Up Zone Total No of Dwellings:		30	
	<i>Survey date: MONDAY</i>		<i>06/07/09</i>	<i>Survey Type: MANUAL</i>

LIST OF SITES relevant to selection parameters (Cont.)

9	GM-03-C-02	BLOCK OF FLATS WHITWORTH STREET W. MANCHESTER		GREATER MANCHESTER
		Town Centre Built-Up Zone Total No of Dwellings:	154	
		<i>Survey date: THURSDAY</i>	<i>13/10/11</i>	<i>Survey Type: MANUAL</i>
10	GM-03-C-03	BLOCK OF FLATS FAIRFIELD STREET MANCHESTER		GREATER MANCHESTER
		Town Centre Built-Up Zone Total No of Dwellings:	20	
		<i>Survey date: FRIDAY</i>	<i>14/10/11</i>	<i>Survey Type: MANUAL</i>
11	HF-03-C-03	BLOCK OF FLATS SHENLEY ROAD BOREHAMWOOD		HERTFORDSHIRE
		Edge of Town Centre Built-Up Zone Total No of Dwellings:	91	
		<i>Survey date: THURSDAY</i>	<i>14/11/19</i>	<i>Survey Type: MANUAL</i>
12	HI-03-C-01	FLATS SHORE STREET INVERNESS		HIGHLAND
		Town Centre Residential Zone Total No of Dwellings:	38	
		<i>Survey date: WEDNESDAY</i>	<i>20/05/09</i>	<i>Survey Type: MANUAL</i>
13	KS-03-C-01	BLOCK OF FLATS KINGS MILL LANE HUDDERSFIELD ASPLEY		KIRKLEES
		Edge of Town Centre Built-Up Zone Total No of Dwellings:	12	
		<i>Survey date: WEDNESDAY</i>	<i>13/09/06</i>	<i>Survey Type: MANUAL</i>
14	LS-03-C-01	BLOCK OF FLATS EAST STREET LEEDS CROWN POINT		LEEDS
		Edge of Town Centre Development Zone Total No of Dwellings:	127	
		<i>Survey date: THURSDAY</i>	<i>13/11/03</i>	<i>Survey Type: MANUAL</i>
15	MS-03-C-01	BLOCKS OF FLATS WAPPING ROAD LIVERPOOL WAPPING DOCK		MERSEYSIDE
		Edge of Town Centre Development Zone Total No of Dwellings:	114	
		<i>Survey date: THURSDAY</i>	<i>16/10/03</i>	<i>Survey Type: MANUAL</i>
16	MS-03-C-04	BLOCK OF FLATS HOY DRIVE NEWTON-LE-WILLOWS EARLESTOWN		MERSEYSIDE
		Edge of Town Centre Residential Zone Total No of Dwellings:	24	
		<i>Survey date: MONDAY</i>	<i>12/04/21</i>	<i>Survey Type: MANUAL</i>

LIST OF SITES relevant to selection parameters (Cont.)

17	NF-03-C-01 PAGE STAIR LANE KING'S LYNN	BLOCKS OF FLATS		NORFOLK
	Edge of Town Centre Built-Up Zone Total No of Dwellings:		51	
	<i>Survey date: THURSDAY</i>		<i>11/12/14</i>	<i>Survey Type: MANUAL</i>
18	PO-03-C-01 CROSS STREET PORTSMOUTH	BLOCKS OF FLATS		PORTSMOUTH
	Edge of Town Centre Built-Up Zone Total No of Dwellings:		90	
	<i>Survey date: TUESDAY</i>		<i>05/06/18</i>	<i>Survey Type: MANUAL</i>
19	SA-03-C-01 RACECOURSE ROAD AYR	BLOCK OF FLATS		SOUTH AYRSHIRE
	Edge of Town Centre Residential Zone Total No of Dwellings:		51	
	<i>Survey date: TUESDAY</i>		<i>16/09/14</i>	<i>Survey Type: MANUAL</i>
20	SC-03-C-01 HEATHCOTE ROAD CAMBERLEY	FLATS		SURREY
	Edge of Town Centre Residential Zone Total No of Dwellings:		140	
	<i>Survey date: MONDAY</i>		<i>21/07/08</i>	<i>Survey Type: MANUAL</i>
21	SF-03-C-01 STATION HILL BURY ST EDMUNDS	BLOCKS OF FLATS		SUFFOLK
	Edge of Town Centre Built-Up Zone Total No of Dwellings:		85	
	<i>Survey date: THURSDAY</i>		<i>18/12/14</i>	<i>Survey Type: MANUAL</i>
22	SF-03-C-05 FORE STREET IPSWICH IPSWICH WATERFRONT	BLOCKS OF FLATS		SUFFOLK
	Edge of Town Centre Development Zone Total No of Dwellings:		69	
	<i>Survey date: WEDNESDAY</i>		<i>23/06/21</i>	<i>Survey Type: MANUAL</i>
23	SR-03-C-01 FORTHESIDE WAY STIRLING	FLATS		STIRLING
	Edge of Town Centre No Sub Category Total No of Dwellings:		80	
	<i>Survey date: WEDNESDAY</i>		<i>18/06/14</i>	<i>Survey Type: MANUAL</i>
24	SR-03-C-02 ROSEBERRY TERRACE STIRLING	FLATS		STIRLING
	Edge of Town Centre Residential Zone Total No of Dwellings:		48	
	<i>Survey date: WEDNESDAY</i>		<i>18/06/14</i>	<i>Survey Type: MANUAL</i>

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LIST OF SITES relevant to selection parameters (Cont.)

25	SS-03-C-01	FLATS		SOUTHEND ON SEA
	WESTCLIFF PARADE			
	SOUTHEND-ON-SEA			
	WESTCLIFF			
	Edge of Town Centre			
	Residential Zone			
	Total No of Dwellings:		6	
	<i>Survey date: TUESDAY</i>		<i>22/10/13</i>	<i>Survey Type: MANUAL</i>
26	SS-03-C-02	BLOCK OF FLATS		SOUTHEND ON SEA
	WESTCLIFF PARADE			
	SOUTHEND-ON-SEA			
	WESTCLIFF			
	Edge of Town Centre			
	Residential Zone			
	Total No of Dwellings:		94	
	<i>Survey date: TUESDAY</i>		<i>22/10/13</i>	<i>Survey Type: MANUAL</i>
27	WM-03-C-03	FLATS		WEST MIDLANDS
	LODE LANE			
	SOLIHULL			
	Edge of Town Centre			
	No Sub Category			
	Total No of Dwellings:		60	
	<i>Survey date: FRIDAY</i>		<i>21/09/07</i>	<i>Survey Type: MANUAL</i>

This section provides a list of all survey sites and days in the selected set. For each individual survey site, it displays a unique site reference code and site address, the selected trip rate calculation parameter and its value, the day of the week and date of each survey, and whether the survey was a manual classified count or an ATC count.

TRIP RATE for Land Use 03 - RESIDENTIAL/C - FLATS PRIVATELY OWNED

MULTI-MODAL TOTAL VEHICLES

Calculation factor: 1 DWELLS

BOLD print indicates peak (busiest) period

Total People to Total Vehicles ratio (all time periods and directions): 2.40

Time Range	ARRIVALS			DEPARTURES			TOTALS		
	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate
00:00 - 01:00									
01:00 - 02:00									
02:00 - 03:00									
03:00 - 04:00									
04:00 - 05:00									
05:00 - 06:00									
06:00 - 07:00									
07:00 - 08:00	27	72	0.033	27	72	0.131	27	72	0.164
08:00 - 09:00	27	72	0.060	27	72	0.182	27	72	0.242
09:00 - 10:00	27	72	0.063	27	72	0.081	27	72	0.144
10:00 - 11:00	27	72	0.069	27	72	0.082	27	72	0.151
11:00 - 12:00	27	72	0.066	27	72	0.080	27	72	0.146
12:00 - 13:00	27	72	0.089	27	72	0.082	27	72	0.171
13:00 - 14:00	27	72	0.069	27	72	0.084	27	72	0.153
14:00 - 15:00	27	72	0.070	27	72	0.080	27	72	0.150
15:00 - 16:00	27	72	0.093	27	72	0.066	27	72	0.159
16:00 - 17:00	27	72	0.119	27	72	0.075	27	72	0.194
17:00 - 18:00	27	72	0.164	27	72	0.093	27	72	0.257
18:00 - 19:00	27	72	0.159	27	72	0.087	27	72	0.246
19:00 - 20:00									
20:00 - 21:00									
21:00 - 22:00									
22:00 - 23:00									
23:00 - 24:00									
Total Rates:			1.054			1.123			2.177

This section displays the trip rate results based on the selected set of surveys and the selected count type (shown just above the table). It is split by three main columns, representing arrivals trips, departures trips, and total trips (arrivals plus departures). Within each of these main columns are three sub-columns. These display the number of survey days where count data is included (per time period), the average value of the selected trip rate calculation parameter (per time period), and the trip rate result (per time period). Total trip rates (the sum of the column) are also displayed at the foot of the table.

To obtain a trip rate, the average (mean) trip rate parameter value (TRP) is first calculated for all selected survey days that have count data available for the stated time period. The average (mean) number of arrivals, departures or totals (whichever applies) is also calculated (COUNT) for all selected survey days that have count data available for the stated time period. Then, the average count is divided by the average trip rate parameter value, and multiplied by the stated calculation factor (shown just above the table and abbreviated here as FACT). So, the method is: $COUNT/TRP*FACT$. Trip rates are then rounded to 3 decimal places.

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Parameter summary

Trip rate parameter range selected: 6 - 175 (units:)
Survey date date range: 01/01/00 - 11/05/22
Number of weekdays (Monday-Friday): 27
Number of Saturdays: 0
Number of Sundays: 0
Surveys automatically removed from selection: 0
Surveys manually removed from selection: 0

This section displays a quick summary of some of the data filtering selections made by the TRICS® user. The trip rate calculation parameter range of all selected surveys is displayed first, followed by the range of minimum and maximum survey dates selected by the user. Then, the total number of selected weekdays and weekend days in the selected set of surveys are shown. Finally, the number of survey days that have been manually removed from the selected set outside of the standard filtering procedure are displayed.

SCP York Street Manchester

Licence No: 726001

TRIP RATE for Land Use 03 - RESIDENTIAL/C - FLATS PRIVATELY OWNED

MULTI-MODAL CYCLISTS

Calculation factor: 1 DWELLS

BOLD print indicates peak (busiest) period

Time Range	ARRIVALS			DEPARTURES			TOTALS		
	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate
00:00 - 01:00									
01:00 - 02:00									
02:00 - 03:00									
03:00 - 04:00									
04:00 - 05:00									
05:00 - 06:00									
06:00 - 07:00									
07:00 - 08:00	27	72	0.002	27	72	0.007	27	72	0.009
08:00 - 09:00	27	72	0.003	27	72	0.009	27	72	0.012
09:00 - 10:00	27	72	0.002	27	72	0.004	27	72	0.006
10:00 - 11:00	27	72	0.003	27	72	0.005	27	72	0.008
11:00 - 12:00	27	72	0.004	27	72	0.003	27	72	0.007
12:00 - 13:00	27	72	0.001	27	72	0.004	27	72	0.005
13:00 - 14:00	27	72	0.002	27	72	0.002	27	72	0.004
14:00 - 15:00	27	72	0.003	27	72	0.001	27	72	0.004
15:00 - 16:00	27	72	0.004	27	72	0.003	27	72	0.007
16:00 - 17:00	27	72	0.002	27	72	0.001	27	72	0.003
17:00 - 18:00	27	72	0.010	27	72	0.003	27	72	0.013
18:00 - 19:00	27	72	0.005	27	72	0.002	27	72	0.007
19:00 - 20:00									
20:00 - 21:00									
21:00 - 22:00									
22:00 - 23:00									
23:00 - 24:00									
Total Rates:			0.041			0.044			0.085

This section displays the trip rate results based on the selected set of surveys and the selected count type (shown just above the table). It is split by three main columns, representing arrivals trips, departures trips, and total trips (arrivals plus departures). Within each of these main columns are three sub-columns. These display the number of survey days where count data is included (per time period), the average value of the selected trip rate calculation parameter (per time period), and the trip rate result (per time period). Total trip rates (the sum of the column) are also displayed at the foot of the table.

To obtain a trip rate, the average (mean) trip rate parameter value (TRP) is first calculated for all selected survey days that have count data available for the stated time period. The average (mean) number of arrivals, departures or totals (whichever applies) is also calculated (COUNT) for all selected survey days that have count data available for the stated time period. Then, the average count is divided by the average trip rate parameter value, and multiplied by the stated calculation factor (shown just above the table and abbreviated here as FACT). So, the method is: $COUNT/TRP*FACT$. Trip rates are then rounded to 3 decimal places.

TRIP RATE for Land Use 03 - RESIDENTIAL/C - FLATS PRIVATELY OWNED

MULTI-MODAL PEDESTRIANS

Calculation factor: 1 DWELLS

BOLD print indicates peak (busiest) period

Time Range	ARRIVALS			DEPARTURES			TOTALS		
	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate
00:00 - 01:00									
01:00 - 02:00									
02:00 - 03:00									
03:00 - 04:00									
04:00 - 05:00									
05:00 - 06:00									
06:00 - 07:00									
07:00 - 08:00	27	72	0.018	27	72	0.066	27	72	0.084
08:00 - 09:00	27	72	0.027	27	72	0.132	27	72	0.159
09:00 - 10:00	27	72	0.040	27	72	0.084	27	72	0.124
10:00 - 11:00	27	72	0.054	27	72	0.060	27	72	0.114
11:00 - 12:00	27	72	0.046	27	72	0.068	27	72	0.114
12:00 - 13:00	27	72	0.073	27	72	0.071	27	72	0.144
13:00 - 14:00	27	72	0.069	27	72	0.063	27	72	0.132
14:00 - 15:00	27	72	0.065	27	72	0.060	27	72	0.125
15:00 - 16:00	27	72	0.081	27	72	0.057	27	72	0.138
16:00 - 17:00	27	72	0.102	27	72	0.082	27	72	0.184
17:00 - 18:00	27	72	0.137	27	72	0.071	27	72	0.208
18:00 - 19:00	27	72	0.095	27	72	0.058	27	72	0.153
19:00 - 20:00									
20:00 - 21:00									
21:00 - 22:00									
22:00 - 23:00									
23:00 - 24:00									
Total Rates:			0.807			0.872			1.679

This section displays the trip rate results based on the selected set of surveys and the selected count type (shown just above the table). It is split by three main columns, representing arrivals trips, departures trips, and total trips (arrivals plus departures). Within each of these main columns are three sub-columns. These display the number of survey days where count data is included (per time period), the average value of the selected trip rate calculation parameter (per time period), and the trip rate result (per time period). Total trip rates (the sum of the column) are also displayed at the foot of the table.

To obtain a trip rate, the average (mean) trip rate parameter value (TRP) is first calculated for all selected survey days that have count data available for the stated time period. The average (mean) number of arrivals, departures or totals (whichever applies) is also calculated (COUNT) for all selected survey days that have count data available for the stated time period. Then, the average count is divided by the average trip rate parameter value, and multiplied by the stated calculation factor (shown just above the table and abbreviated here as FACT). So, the method is: $COUNT/TRP*FACT$. Trip rates are then rounded to 3 decimal places.

TRIP RATE for Land Use 03 - RESIDENTIAL/C - FLATS PRIVATELY OWNED
MULTI-MODAL PUBLIC TRANSPORT USERS

Calculation factor: 1 DWELLS

BOLD print indicates peak (busiest) period

Time Range	ARRIVALS			DEPARTURES			TOTALS		
	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate
00:00 - 01:00									
01:00 - 02:00									
02:00 - 03:00									
03:00 - 04:00									
04:00 - 05:00									
05:00 - 06:00									
06:00 - 07:00									
07:00 - 08:00	27	72	0.001	27	72	0.054	27	72	0.055
08:00 - 09:00	27	72	0.005	27	72	0.084	27	72	0.089
09:00 - 10:00	27	72	0.005	27	72	0.028	27	72	0.033
10:00 - 11:00	27	72	0.011	27	72	0.012	27	72	0.023
11:00 - 12:00	27	72	0.015	27	72	0.011	27	72	0.026
12:00 - 13:00	27	72	0.018	27	72	0.020	27	72	0.038
13:00 - 14:00	27	72	0.011	27	72	0.022	27	72	0.033
14:00 - 15:00	27	72	0.017	27	72	0.008	27	72	0.025
15:00 - 16:00	27	72	0.045	27	72	0.011	27	72	0.056
16:00 - 17:00	27	72	0.035	27	72	0.011	27	72	0.046
17:00 - 18:00	27	72	0.064	27	72	0.008	27	72	0.072
18:00 - 19:00	27	72	0.039	27	72	0.007	27	72	0.046
19:00 - 20:00									
20:00 - 21:00									
21:00 - 22:00									
22:00 - 23:00									
23:00 - 24:00									
Total Rates:			0.266			0.276			0.542

This section displays the trip rate results based on the selected set of surveys and the selected count type (shown just above the table). It is split by three main columns, representing arrivals trips, departures trips, and total trips (arrivals plus departures). Within each of these main columns are three sub-columns. These display the number of survey days where count data is included (per time period), the average value of the selected trip rate calculation parameter (per time period), and the trip rate result (per time period). Total trip rates (the sum of the column) are also displayed at the foot of the table.

*To obtain a trip rate, the average (mean) trip rate parameter value (TRP) is first calculated for all selected survey days that have count data available for the stated time period. The average (mean) number of arrivals, departures or totals (whichever applies) is also calculated (COUNT) for all selected survey days that have count data available for the stated time period. Then, the average count is divided by the average trip rate parameter value, and multiplied by the stated calculation factor (shown just above the table and abbreviated here as FACT). So, the method is: COUNT/TRP*FACT. Trip rates are then rounded to 3 decimal places.*

TRIP RATE CALCULATION SELECTION PARAMETERS:

Land Use : 03 - RESIDENTIAL
Category : A - HOUSES PRIVATELY OWNED
MULTI-MODAL TOTAL VEHICLES

Selected regions and areas:

02	SOUTH EAST	
	HC HAMPSHIRE	1 days
	HF HERTFORDSHIRE	1 days
	IW ISLE OF WIGHT	1 days
03	SOUTH WEST	
	GS GLOUCESTERSHIRE	1 days
04	EAST ANGLIA	
	PB PETERBOROUGH	1 days
05	EAST MIDLANDS	
	LN LINCOLNSHIRE	1 days
06	WEST MIDLANDS	
	ST STAFFORDSHIRE	1 days
	WM WEST MIDLANDS	1 days
07	YORKSHIRE & NORTH LINCOLNSHIRE	
	NE NORTH EAST LINCOLNSHIRE	1 days
	NY NORTH YORKSHIRE	2 days
08	NORTH WEST	
	BP BLACKPOOL	1 days
09	NORTH	
	FU WESTMORLAND & FURNESS	1 days
10	WALES	
	PS POWYS	1 days

This section displays the number of survey days per TRICS® sub-region in the selected set

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Primary Filtering selection:

This data displays the chosen trip rate parameter and its selected range. Only sites that fall within the parameter range are included in the trip rate calculation.

Parameter: No of Dwellings
Actual Range: 14 to 363 (units:)
Range Selected by User: 6 to 1817 (units:)

Parking Spaces Range: All Surveys Included

Parking Spaces per Dwelling Range: All Surveys Included

Bedrooms per Dwelling Range: All Surveys Included

Percentage of dwellings privately owned: All Surveys Included

Public Transport Provision:

Selection by: Include all surveys

Date Range: 01/01/00 to 29/06/23

This data displays the range of survey dates selected. Only surveys that were conducted within this date range are included in the trip rate calculation.

Selected survey days:

Monday	4 days
Tuesday	5 days
Thursday	1 days
Friday	4 days

This data displays the number of selected surveys by day of the week.

Selected survey types:

Manual count	14 days
Directional ATC Count	0 days

This data displays the number of manual classified surveys and the number of unclassified ATC surveys, the total adding up to the overall number of surveys in the selected set. Manual surveys are undertaken using staff, whilst ATC surveys are undertaken using machines.

Selected Locations:

Edge of Town Centre	13
Free Standing (PPS6 Out of Town)	1

This data displays the number of surveys per main location category within the selected set. The main location categories consist of Free Standing, Edge of Town, Suburban Area, Neighbourhood Centre, Edge of Town Centre, Town Centre and Not Known.

Selected Location Sub Categories:

Residential Zone	11
Out of Town	1
No Sub Category	2

This data displays the number of surveys per location sub-category within the selected set. The location sub-categories consist of Commercial Zone, Industrial Zone, Development Zone, Residential Zone, Retail Zone, Built-Up Zone, Village, Out of Town, High Street and No Sub Category.

Inclusion of Servicing Vehicles Counts:

Servicing vehicles Included	2 days - Selected
Servicing vehicles Excluded	12 days - Selected

Secondary Filtering selection:

Use Class:

C3	14 days
----	---------

This data displays the number of surveys per Use Class classification within the selected set. The Use Classes Order (England) 2020 has been used for this purpose, which can be found within the Library module of TRICS®.

Population within 500m Range:

All Surveys Included

Secondary Filtering selection (Cont.):

Population within 1 mile:

1,001 to 5,000	2 days
5,001 to 10,000	3 days
10,001 to 15,000	3 days
15,001 to 20,000	1 days
20,001 to 25,000	2 days
25,001 to 50,000	3 days

This data displays the number of selected surveys within stated 1-mile radii of population.

Population within 5 miles:

5,001 to 25,000	5 days
25,001 to 50,000	1 days
50,001 to 75,000	1 days
100,001 to 125,000	1 days
125,001 to 250,000	2 days
250,001 to 500,000	4 days

This data displays the number of selected surveys within stated 5-mile radii of population.

Car ownership within 5 miles:

0.5 or Less	1 days
0.6 to 1.0	4 days
1.1 to 1.5	8 days
1.6 to 2.0	1 days

This data displays the number of selected surveys within stated ranges of average cars owned per residential dwelling, within a radius of 5-miles of selected survey sites.

Travel Plan:

Not Known	1 days
Yes	1 days
No	12 days

This data displays the number of surveys within the selected set that were undertaken at sites with Travel Plans in place, and the number of surveys that were undertaken at sites without Travel Plans.

PTAL Rating:

No PTAL Present	14 days
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This data displays the number of selected surveys with PTAL Ratings.

LIST OF SITES relevant to selection parameters

1	BP-03-A-01 WATSON ROAD BLACKPOOL	SEMI -DETACHED		BLACKPOOL
	Edge of Town Centre Residential Zone Total No of Dwellings:		24	
	<i>Survey date: FRIDAY</i>		<i>14/06/13</i>	<i>Survey Type: MANUAL</i>
2	FU-03-A-02 MACADAM WAY PENRITH	DETACHED/TERRACED HOUSING		WESTMORLAND & FURNESS
	Edge of Town Centre Residential Zone Total No of Dwellings:		50	
	<i>Survey date: TUESDAY</i>		<i>21/06/16</i>	<i>Survey Type: MANUAL</i>
3	GS-03-A-01 KINGSHOLM ROAD GLOUCESTER KINGSHOLM	SEMI D./TERRACED		GLOUCESTERSHIRE
	Edge of Town Centre No Sub Category Total No of Dwellings:		73	
	<i>Survey date: TUESDAY</i>		<i>25/05/04</i>	<i>Survey Type: MANUAL</i>
4	HC-03-A-30 MEUDON AVENUE FARNBOROUGH	TERRACED HOUSES		HAMPSHIRE
	Edge of Town Centre Residential Zone Total No of Dwellings:		31	
	<i>Survey date: FRIDAY</i>		<i>14/10/22</i>	<i>Survey Type: MANUAL</i>
5	HF-03-A-01 LONGCROFT LANE WELWYN GARDEN CITY	MIXED HOUSES		HERTFORDSHIRE
	Edge of Town Centre Residential Zone Total No of Dwellings:		53	
	<i>Survey date: FRIDAY</i>		<i>06/09/02</i>	<i>Survey Type: MANUAL</i>
6	IW-03-A-01 MEDHAM FARM LANE NEAR COWES MEDHAM	DETACHED HOUSES		ISLE OF WIGHT
	Free Standing (PPS6 Out of Town) Out of Town Total No of Dwellings:		72	
	<i>Survey date: TUESDAY</i>		<i>25/06/19</i>	<i>Survey Type: MANUAL</i>
7	LN-03-A-04 EGERTON ROAD LINCOLN	DETACHED & SEMI -DETACHED		LINCOLNSHIRE
	Edge of Town Centre Residential Zone Total No of Dwellings:		30	
	<i>Survey date: MONDAY</i>		<i>29/06/15</i>	<i>Survey Type: MANUAL</i>
8	NE-03-A-03 STATION ROAD SCUNTHORPE	PRIVATE HOUSES		NORTH EAST LINCOLNSHIRE
	Edge of Town Centre Residential Zone Total No of Dwellings:		180	
	<i>Survey date: TUESDAY</i>		<i>20/05/14</i>	<i>Survey Type: MANUAL</i>

LIST OF SITES relevant to selection parameters (Cont.)

9	NY-03-A-03 NEW ROW BOROUGHBRIDGE	PRIVATE HOUSING		NORTH YORKSHIRE
	Edge of Town Centre Residential Zone Total No of Dwellings:		14	
	Survey date: MONDAY		15/09/08	Survey Type: MANUAL
10	NY-03-A-12 RACECOURSE LANE NORTHALLERTON	TOWN HOUSES		NORTH YORKSHIRE
	Edge of Town Centre Residential Zone Total No of Dwellings:		47	
	Survey date: TUESDAY		27/09/16	Survey Type: MANUAL
11	PB-03-A-01 THORPE ROAD PETERBOROUGH	MIXED HOUSES		PETERBOROUGH
	Edge of Town Centre Residential Zone Total No of Dwellings:		363	
	Survey date: THURSDAY		13/05/04	Survey Type: MANUAL
12	PS-03-A-01 BRYN GLAS WELSHPOOL	MIXED HOUSES		POWYS
	Edge of Town Centre Residential Zone Total No of Dwellings:		16	
	Survey date: MONDAY		11/05/15	Survey Type: MANUAL
13	ST-03-A-06 STANFORD ROAD WOLVERHAMPTON BLAKENHALL	SEMI-DET. & TERRACED		STAFFORDSHIRE
	Edge of Town Centre No Sub Category Total No of Dwellings:		17	
	Survey date: FRIDAY		09/05/14	Survey Type: MANUAL
14	WM-03-A-05 COUNDON ROAD COVENTRY	TERRACED & DETACHED		WEST MIDLANDS
	Edge of Town Centre Residential Zone Total No of Dwellings:		89	
	Survey date: MONDAY		21/11/16	Survey Type: MANUAL

This section provides a list of all survey sites and days in the selected set. For each individual survey site, it displays a unique site reference code and site address, the selected trip rate calculation parameter and its value, the day of the week and date of each survey, and whether the survey was a manual classified count or an ATC count.

TRIP RATE for Land Use 03 - RESIDENTIAL/A - HOUSES PRIVATELY OWNED

MULTI-MODAL TOTAL VEHICLES

Calculation factor: 1 DWELLS

BOLD print indicates peak (busiest) period

Total People to Total Vehicles ratio (all time periods and directions): 1.83

Time Range	ARRIVALS			DEPARTURES			TOTALS		
	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate
00:00 - 01:00									
01:00 - 02:00									
02:00 - 03:00									
03:00 - 04:00									
04:00 - 05:00									
05:00 - 06:00									
06:00 - 07:00									
07:00 - 08:00	14	76	0.055	14	76	0.164	14	76	0.219
08:00 - 09:00	14	76	0.162	14	76	0.310	14	76	0.472
09:00 - 10:00	14	76	0.159	14	76	0.129	14	76	0.288
10:00 - 11:00	14	76	0.122	14	76	0.142	14	76	0.264
11:00 - 12:00	14	76	0.138	14	76	0.127	14	76	0.265
12:00 - 13:00	14	76	0.170	14	76	0.148	14	76	0.318
13:00 - 14:00	14	76	0.161	14	76	0.154	14	76	0.315
14:00 - 15:00	14	76	0.148	14	76	0.168	14	76	0.316
15:00 - 16:00	14	76	0.184	14	76	0.167	14	76	0.351
16:00 - 17:00	14	76	0.210	14	76	0.161	14	76	0.371
17:00 - 18:00	14	76	0.254	14	76	0.179	14	76	0.433
18:00 - 19:00	14	76	0.179	14	76	0.167	14	76	0.346
19:00 - 20:00									
20:00 - 21:00									
21:00 - 22:00									
22:00 - 23:00									
23:00 - 24:00									
Total Rates:			1.942			2.016			3.958

This section displays the trip rate results based on the selected set of surveys and the selected count type (shown just above the table). It is split by three main columns, representing arrivals trips, departures trips, and total trips (arrivals plus departures). Within each of these main columns are three sub-columns. These display the number of survey days where count data is included (per time period), the average value of the selected trip rate calculation parameter (per time period), and the trip rate result (per time period). Total trip rates (the sum of the column) are also displayed at the foot of the table.

To obtain a trip rate, the average (mean) trip rate parameter value (TRP) is first calculated for all selected survey days that have count data available for the stated time period. The average (mean) number of arrivals, departures or totals (whichever applies) is also calculated (COUNT) for all selected survey days that have count data available for the stated time period. Then, the average count is divided by the average trip rate parameter value, and multiplied by the stated calculation factor (shown just above the table and abbreviated here as FACT). So, the method is: $COUNT/TRP*FACT$. Trip rates are then rounded to 3 decimal places.

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Parameter summary

Trip rate parameter range selected: 14 - 363 (units:)
Survey date date range: 01/01/00 - 29/06/23
Number of weekdays (Monday-Friday): 14
Number of Saturdays: 0
Number of Sundays: 0
Surveys automatically removed from selection: 0
Surveys manually removed from selection: 0

This section displays a quick summary of some of the data filtering selections made by the TRICS® user. The trip rate calculation parameter range of all selected surveys is displayed first, followed by the range of minimum and maximum survey dates selected by the user. Then, the total number of selected weekdays and weekend days in the selected set of surveys are shown. Finally, the number of survey days that have been manually removed from the selected set outside of the standard filtering procedure are displayed.

SCP York Street Manchester

Licence No: 726001

TRIP RATE for Land Use 03 - RESIDENTIAL/A - HOUSES PRIVATELY OWNED

MULTI-MODAL CYCLISTS

Calculation factor: 1 DWELLS

BOLD print indicates peak (busiest) period

Time Range	ARRIVALS			DEPARTURES			TOTALS		
	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate
00:00 - 01:00									
01:00 - 02:00									
02:00 - 03:00									
03:00 - 04:00									
04:00 - 05:00									
05:00 - 06:00									
06:00 - 07:00									
07:00 - 08:00	14	76	0.003	14	76	0.018	14	76	0.021
08:00 - 09:00	14	76	0.004	14	76	0.019	14	76	0.023
09:00 - 10:00	14	76	0.004	14	76	0.006	14	76	0.010
10:00 - 11:00	14	76	0.005	14	76	0.003	14	76	0.008
11:00 - 12:00	14	76	0.005	14	76	0.004	14	76	0.009
12:00 - 13:00	14	76	0.003	14	76	0.002	14	76	0.005
13:00 - 14:00	14	76	0.002	14	76	0.004	14	76	0.006
14:00 - 15:00	14	76	0.008	14	76	0.008	14	76	0.016
15:00 - 16:00	14	76	0.013	14	76	0.005	14	76	0.018
16:00 - 17:00	14	76	0.008	14	76	0.006	14	76	0.014
17:00 - 18:00	14	76	0.019	14	76	0.007	14	76	0.026
18:00 - 19:00	14	76	0.009	14	76	0.006	14	76	0.015
19:00 - 20:00									
20:00 - 21:00									
21:00 - 22:00									
22:00 - 23:00									
23:00 - 24:00									
Total Rates:			0.083			0.088			0.171

This section displays the trip rate results based on the selected set of surveys and the selected count type (shown just above the table). It is split by three main columns, representing arrivals trips, departures trips, and total trips (arrivals plus departures). Within each of these main columns are three sub-columns. These display the number of survey days where count data is included (per time period), the average value of the selected trip rate calculation parameter (per time period), and the trip rate result (per time period). Total trip rates (the sum of the column) are also displayed at the foot of the table.

To obtain a trip rate, the average (mean) trip rate parameter value (TRP) is first calculated for all selected survey days that have count data available for the stated time period. The average (mean) number of arrivals, departures or totals (whichever applies) is also calculated (COUNT) for all selected survey days that have count data available for the stated time period. Then, the average count is divided by the average trip rate parameter value, and multiplied by the stated calculation factor (shown just above the table and abbreviated here as FACT). So, the method is: $COUNT/TRP*FACT$. Trip rates are then rounded to 3 decimal places.

TRIP RATE for Land Use 03 - RESIDENTIAL/A - HOUSES PRIVATELY OWNED

MULTI-MODAL PEDESTRIANS

Calculation factor: 1 DWELLS

BOLD print indicates peak (busiest) period

Time Range	ARRIVALS			DEPARTURES			TOTALS		
	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate
00:00 - 01:00									
01:00 - 02:00									
02:00 - 03:00									
03:00 - 04:00									
04:00 - 05:00									
05:00 - 06:00									
06:00 - 07:00									
07:00 - 08:00	14	76	0.018	14	76	0.058	14	76	0.076
08:00 - 09:00	14	76	0.029	14	76	0.167	14	76	0.196
09:00 - 10:00	14	76	0.046	14	76	0.067	14	76	0.113
10:00 - 11:00	14	76	0.039	14	76	0.062	14	76	0.101
11:00 - 12:00	14	76	0.047	14	76	0.063	14	76	0.110
12:00 - 13:00	14	76	0.054	14	76	0.047	14	76	0.101
13:00 - 14:00	14	76	0.057	14	76	0.074	14	76	0.131
14:00 - 15:00	14	76	0.059	14	76	0.070	14	76	0.129
15:00 - 16:00	14	76	0.093	14	76	0.069	14	76	0.162
16:00 - 17:00	14	76	0.113	14	76	0.060	14	76	0.173
17:00 - 18:00	14	76	0.129	14	76	0.058	14	76	0.187
18:00 - 19:00	14	76	0.057	14	76	0.035	14	76	0.092
19:00 - 20:00									
20:00 - 21:00									
21:00 - 22:00									
22:00 - 23:00									
23:00 - 24:00									
Total Rates:			0.741			0.830			1.571

This section displays the trip rate results based on the selected set of surveys and the selected count type (shown just above the table). It is split by three main columns, representing arrivals trips, departures trips, and total trips (arrivals plus departures). Within each of these main columns are three sub-columns. These display the number of survey days where count data is included (per time period), the average value of the selected trip rate calculation parameter (per time period), and the trip rate result (per time period). Total trip rates (the sum of the column) are also displayed at the foot of the table.

To obtain a trip rate, the average (mean) trip rate parameter value (TRP) is first calculated for all selected survey days that have count data available for the stated time period. The average (mean) number of arrivals, departures or totals (whichever applies) is also calculated (COUNT) for all selected survey days that have count data available for the stated time period. Then, the average count is divided by the average trip rate parameter value, and multiplied by the stated calculation factor (shown just above the table and abbreviated here as FACT). So, the method is: $COUNT/TRP*FACT$. Trip rates are then rounded to 3 decimal places.

TRIP RATE for Land Use 03 - RESIDENTIAL/A - HOUSES PRIVATELY OWNED

MULTI-MODAL PUBLIC TRANSPORT USERS

Calculation factor: 1 DWELLS

BOLD print indicates peak (busiest) period

Time Range	ARRIVALS			DEPARTURES			TOTALS		
	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate
00:00 - 01:00									
01:00 - 02:00									
02:00 - 03:00									
03:00 - 04:00									
04:00 - 05:00									
05:00 - 06:00									
06:00 - 07:00									
07:00 - 08:00	14	76	0.002	14	76	0.016	14	76	0.018
08:00 - 09:00	14	76	0.002	14	76	0.019	14	76	0.021
09:00 - 10:00	14	76	0.008	14	76	0.009	14	76	0.017
10:00 - 11:00	14	76	0.010	14	76	0.012	14	76	0.022
11:00 - 12:00	14	76	0.007	14	76	0.017	14	76	0.024
12:00 - 13:00	14	76	0.008	14	76	0.006	14	76	0.014
13:00 - 14:00	14	76	0.009	14	76	0.009	14	76	0.018
14:00 - 15:00	14	76	0.008	14	76	0.006	14	76	0.014
15:00 - 16:00	14	76	0.009	14	76	0.006	14	76	0.015
16:00 - 17:00	14	76	0.009	14	76	0.008	14	76	0.017
17:00 - 18:00	14	76	0.018	14	76	0.008	14	76	0.026
18:00 - 19:00	14	76	0.010	14	76	0.003	14	76	0.013
19:00 - 20:00									
20:00 - 21:00									
21:00 - 22:00									
22:00 - 23:00									
23:00 - 24:00									
Total Rates:			0.100			0.119			0.219

This section displays the trip rate results based on the selected set of surveys and the selected count type (shown just above the table). It is split by three main columns, representing arrivals trips, departures trips, and total trips (arrivals plus departures). Within each of these main columns are three sub-columns. These display the number of survey days where count data is included (per time period), the average value of the selected trip rate calculation parameter (per time period), and the trip rate result (per time period). Total trip rates (the sum of the column) are also displayed at the foot of the table.

To obtain a trip rate, the average (mean) trip rate parameter value (TRP) is first calculated for all selected survey days that have count data available for the stated time period. The average (mean) number of arrivals, departures or totals (whichever applies) is also calculated (COUNT) for all selected survey days that have count data available for the stated time period. Then, the average count is divided by the average trip rate parameter value, and multiplied by the stated calculation factor (shown just above the table and abbreviated here as FACT). So, the method is: $COUNT/TRP*FACT$. Trip rates are then rounded to 3 decimal places.