

**Helix Transport Consultants**

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Stonehouse  
Gloucestershire  
GL10 2JF

Planning Services  
Stroud District Council  
Ebley Mill,  
Westward Rd,  
Ebley,  
Stroud  
GL5 4UB

9 January 2023

Dear Sir / Madam,

**Site: Land to the rear of No.5 The Green, Uley, Gloucestershire**

**Development: Improved vehicular access and parking area and associated landscaping**

Helix Transport Consultants Ltd are appointed to provide information in support of the proposed car parking on land to the rear of No. 5 The Green Uley. The new facility will provide off-street parking for No 3, 4 and 5 The Green.

The current application follows on from a planning refusal for a similar development considered under the Stroud District Council Planning Reference S.23/1552/FUL. That application was refused on three grounds relating to the lack of information.

This document concerns itself with issues relating to traffic and transportation and sets out how the current proposals address the previous concerns. The reason for refusing S.23/1552/FUL, relevant to highways, reflects the statutory consultee response from the highway authority which is summarised below:

*The Manual for Gloucestershire Streets states new shared private accesses shall be a minimum of 4.1 metres wide within 15 metres of the behind the back of the carriageway to allow two vehicles to enter and leave simultaneously.*

*Given the intensification associated with the 3 detached dwellings associated vehicle parking being relocated via the existing track to The Rectory and occasional church parking, it is required that the access track demonstrates two-way visible passing space from The Street to the new entrance.....*

*.... two-way passing width is not only required immediately off The Street and at the new car park entrance from The Rectory track edge but also from the track up to The Rectory to where it widens into the parking and turning space.*

*Drainage to prevent run-off across the intensified access track onto the footway and into The Street would be sought if suitable passing space can be demonstrated.*

### **Access Width and Passing Opportunities**

The highway authority has, in their response to S.23/1552/FUL, identified their minimum width requirement for the access to be 4.1m for the first 15m. This is to allow the two-way passage of cars at the entrance. Further comments indicate that, beyond the first 15m a single lane width would suffice so long as suitable passing opportunities are provided.

The highway authority recommends passing spaces at the entrance to the church parking and at the proposed site entrance. In effect, their comments recommend a two-way road between the Street and the site.

Accordingly, the current proposals, shown on the enclosed drawing 2330DWG01 rev A, differ from the previous proposals by allowing for a 4.1m wide surface all the way to the site entrance. This addresses all previous comments relating to the provision of passing spaces and the inter-visibility thereof.

### **Drainage**

At present, surface water run-off from the existing access road will run, uninterrupted, from the private road onto the public footpath. The steep verge between the metalled surface and the Church would also likely channel heavy rainfall from the verge onto the footpath.

The proposed widening of the access road will increase the impermeable area marginally and the highway authority is keen that this doesn't lead to an increase in surface run-off over the public highway.

In order to address the concern, the improvement proposals allow for an infiltration trench alongside the widened accessway. Proposed resurfacing works will re-grade the access so that the surface run-off is tipped into the infiltration trench.

The proposals for intercepting the surface run-off can be expected to achieve significantly more than simply nil-detriment.

### **Parking**

While not specifically raised in the consideration of S.23/1552/FUL, it should be noted that the current proposals differ from the previous submission by providing car parking spaces that meet the Manual for Gloucestershire streets requirement for residential parking. Specifically, by the introduction of a 1.2m wide gap between spaces to allow circulation space around a vehicle.

## **Highway Safety**

The relevant reason for refusing S.23/1552/FUL, states that:

*The proposal would increase trip generation along an existing access track onto a classified road. The plans submitted do not show safe passing of vehicles is possible along the access track. The proposal can not ensure highway safety, including pedestrians, and is considered contrary to policy ES3 criterion 5 and HC1 criterion 9 of the Stroud Local Plan 2015.*

The above reflects the deficiencies inherent in the previous application whereby the potential need for vehicles travelling in opposite directions along the single lane access to reverse, putting vulnerable road users at risk, would be exacerbated by the proposed intensification of use.

Unlike the previous application, the current proposals include a modest widening of the access so as to meet the design guidance contained in the Manual for Gloucestershire Streets. In particular, the proposals are consistent with those set out for 'Private Shared Drives.'

By allowing the two-way passage of vehicles, the current proposals eliminate the potential for hazardous vehicle reversing manoeuvres. This measure will benefit new and existing users alike and, as such, offers betterment in highway safety terms.

## **Cumulative Impacts**

The highway authority's statutory consultee response to application S.23/1552/FUL, acknowledges that:

*It is accepted the off-street parking will not likely significantly increase traffic in the locality serving existing dwellings...*

The dwellings proposed to be served by the parking already exist. These existing residents park their vehicles alongside The Street or on the verge. The associated vehicle movements, therefore, also already exist on the road network. The proposed parking will not, in itself, generate new vehicle trips.

Accordingly, it is concluded that there will be no cumulative impacts on the road network resulting from the proposed development.

## **Summary and Conclusions**

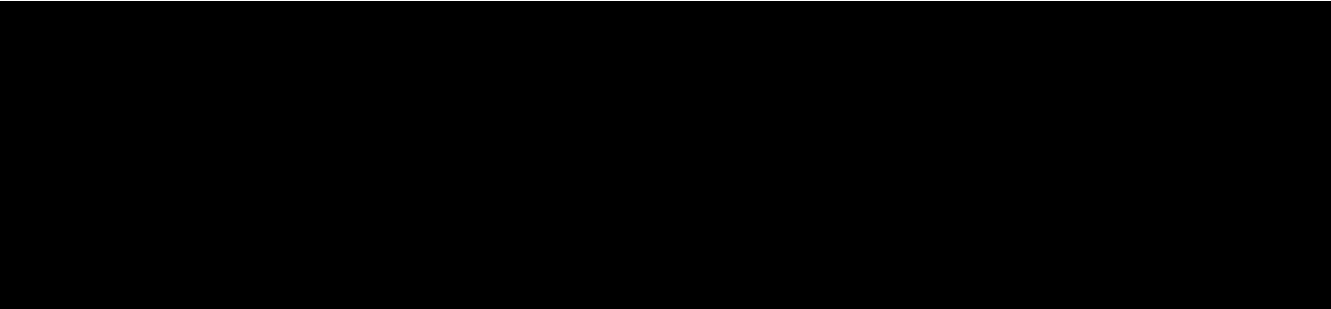
The response to planning application S.23/1552/FUL identified areas of concern over the proposed intensification of use of the existing access between The Green and the site. The current proposals, set out in this letter and the accompanying drawing 2330DWG01A, address these concerns.

The current proposal allows for the widening of the existing access rack to allow the two-way passage of cars. In this way, the potential for dangers associated with vehicles reversing are eliminated.

As part of the resurfacing work, the access road will be tilted slightly towards an infiltration trench. This addresses concerns associated with an increase in surface water run-off discharging into the public highway.

The overall conclusion is that the proposed development will not result in an unacceptable impact on highway safety, or severe residual cumulative impacts on the road network.

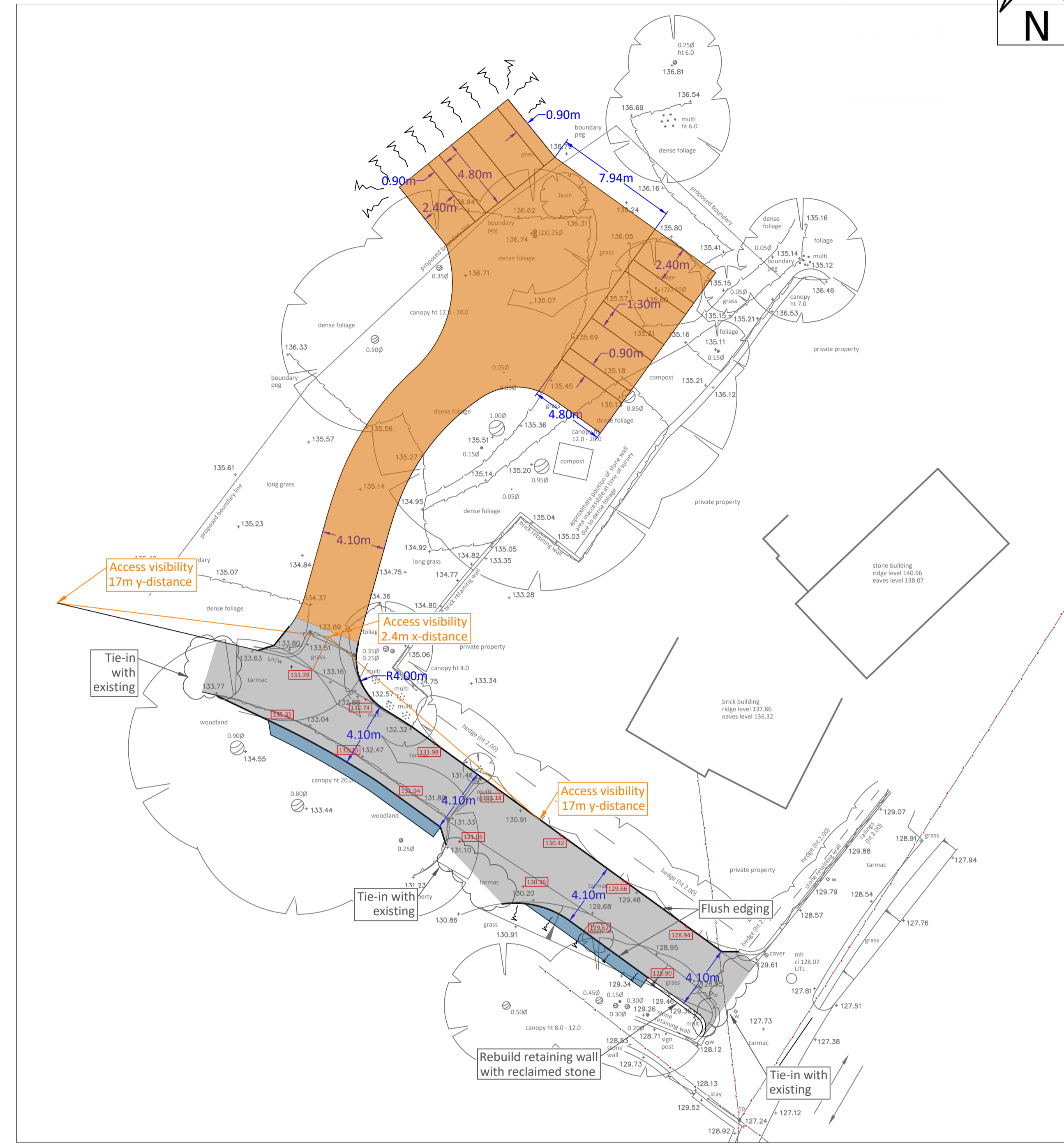
Yours faithfully



# General Arrangement



# General Arrangement showing key dimensions and Features



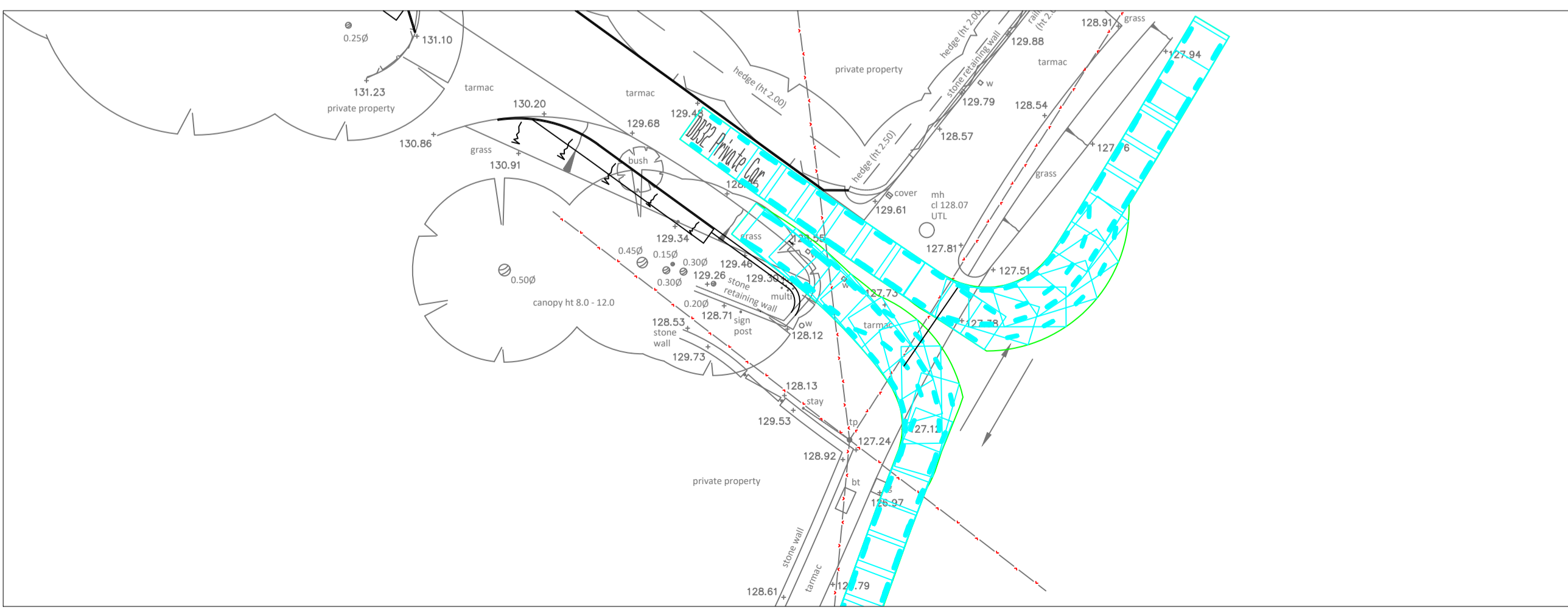
Notes  
Based on topographical survey provided by applicant.

- Key
- Topographical survey base
  - Proposed access layout
  - Proposed access visibility lines
  - Proposed access levels
  - Asphalt surface
  - Free draining hoggin or similar
  - Rainwater run-off infiltration trench

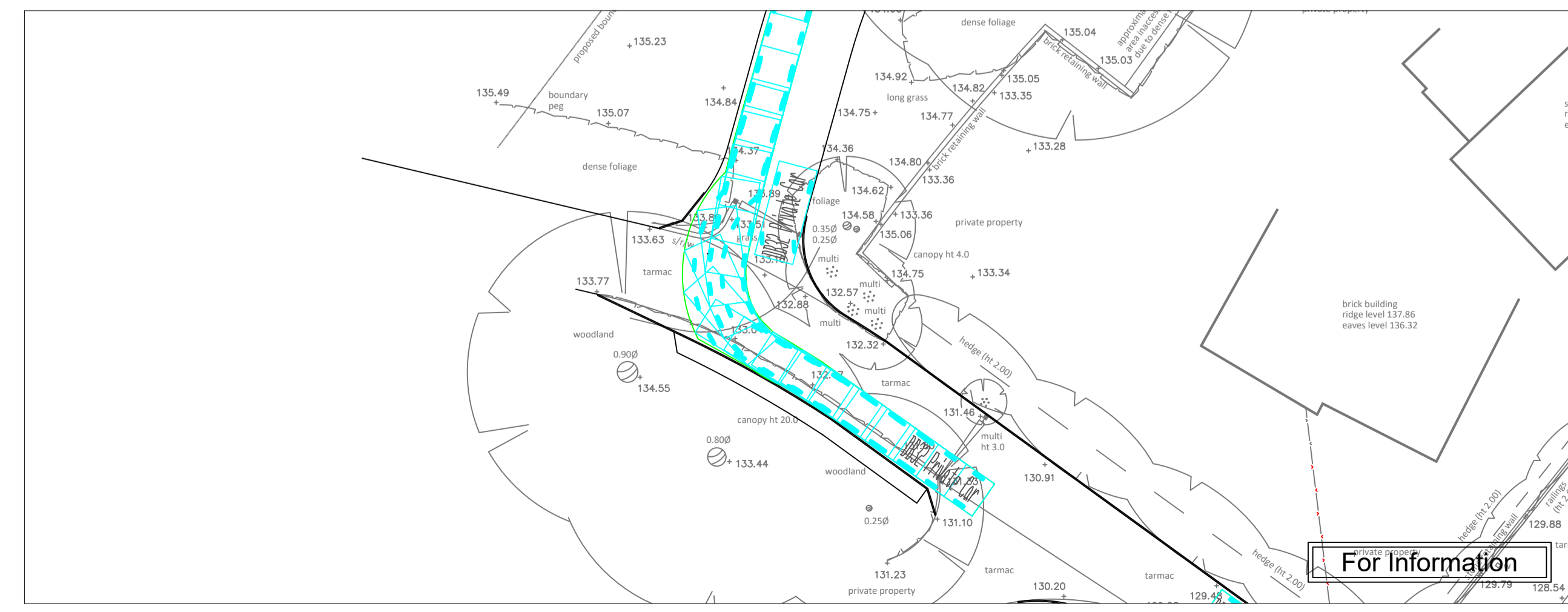
Vehicle Details

DB32 Private Car	4.223m
Overall Length	1.715m
Overall Body Height	1.392m
Min Body Ground Clearance	0.233m
Max Track Width	1.629m
Lock to lock time	4.00s
Kerb to Kerb Turning Radius	5.780m

# Vehicle Manoeuvres at the Junction with The Street



# Vehicle Manoeuvres at the Site Entrance



Rev	By	Chkd	Apprv	Date	Description
A	JH	JH	JH	08/11/23	Details added

Client: [Redacted]

Project: No. 5 The Green Uley

Drawing: Plan Showing Indicative Access Improvements, Key Dimensions Features and Vehicle Manoeuvres

Drawn by: JH	Date: 07/11/2023
Checked by: JH	Date: 07/11/2023
Approved by: JH	Date: 07/11/2023

Drawing No. 2330DWG01 Revision A

Drawing Scale: 1:200@A1