



DESIGN AND ACCESS STATEMENT

Conversion Of The Old Station Yard, Station Road, Narborough,
Leicestershire, LE19 2HR

ON BEHALF OF

JJ Scaffolding

BY

CORPORATE ARCHITECTURE LTD

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1. FOREWORD

- 1.01 The purpose of this statement is to set out the context for, and to explain the design principles behind, the planning application for the change of use from storage/workshop into first floor offices and ground floor Storage to The Old Station Yard building at Station Road Narborough. Leicestershire, Blaby (The District Authority) require a Design & Access statement with accompanying plans prepared by their agents Corporate Architecture Ltd. To the building being within the Narborough Conservation Area.
- 1.01 The building referenced to as the Old Station Yard is a detached building and is two storey with a mezzanine floor forming the first floor. The building is in a prominent position to the historic station yard and adjacent railway station to the North
- 1.02 The applicant is the managing director of JJ Scaffolding and site freeholder which uses the historic station yard for their primary business operation with a company Skill Stone, operating from the station yard building parallel to Station Road.
- 1.03 The building known as The Old Station Yard shed has prominent brick arches and is currently used as a storage warehouse for various machinery. The Applicant proposes to bring the historic building back to life by fully restoring and refurbishment, changing the building use and using this as their new office described later in this statement for his business to operate from.

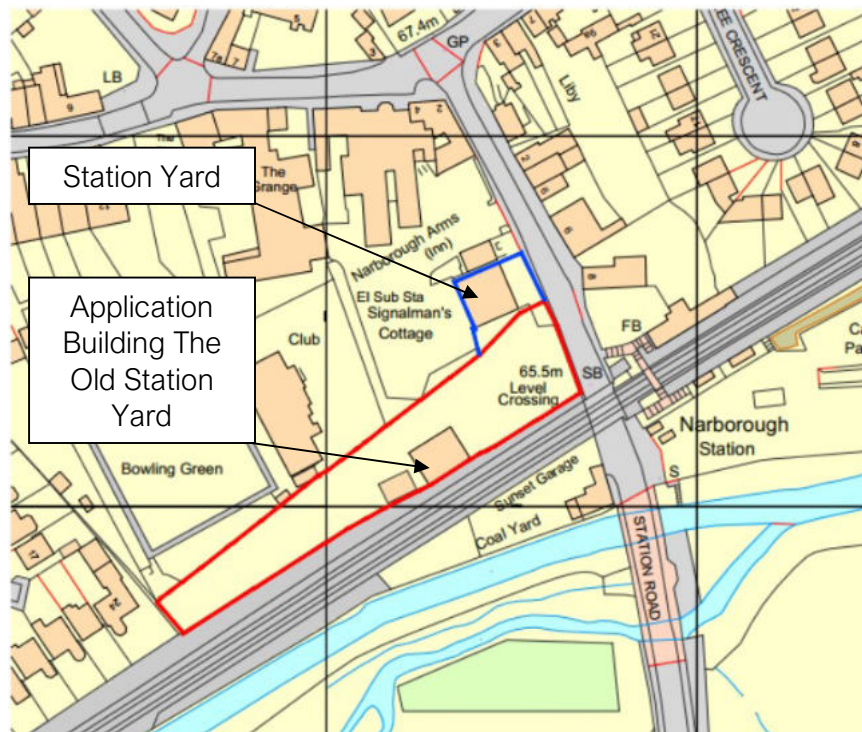


Fig1.0- Location Plan (Location of the site Indicated In Red) From Historic England

1.04 The application building is constructed in solid facing brickwork with brick buttresses. Various brick corner and mid panel buttresses which support a slate roof with indicates where a large central roof lantern used to be. There is a large central opening on the North Western Elevation. A metal staircase provides access to a metal mezzanine floor structure and intricate timber framework and trussed provides support for the existing roof structure. The intention is for this structure is to be retained and made a feature within the proposed offices for the client's business.



Fig1.1– Aerial Plan (Location of the site Indicated In Red) From Goole Maps

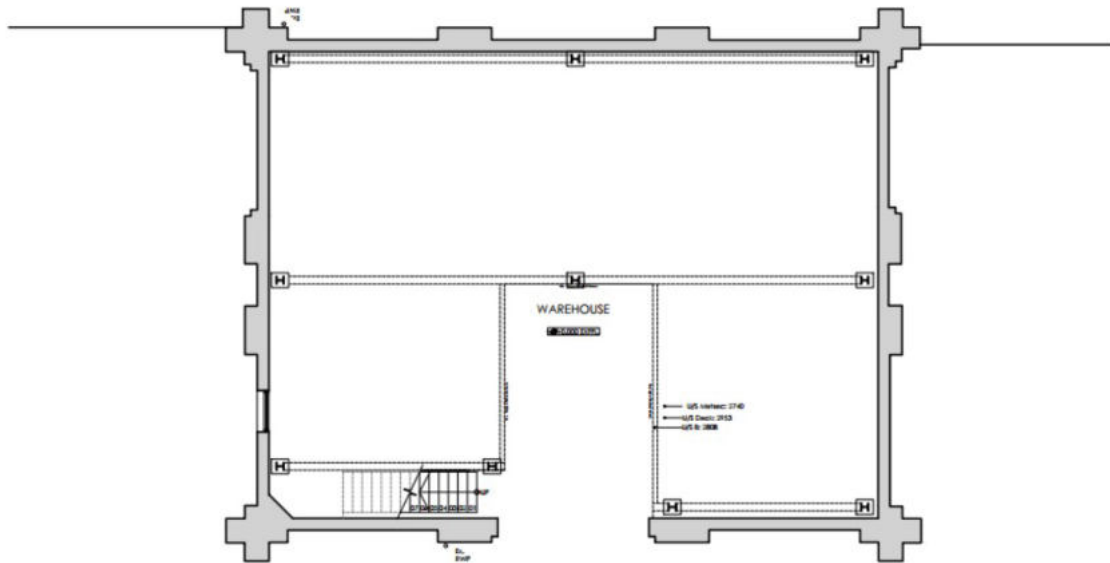


Fig 1.2 Current Ground Floor Plan

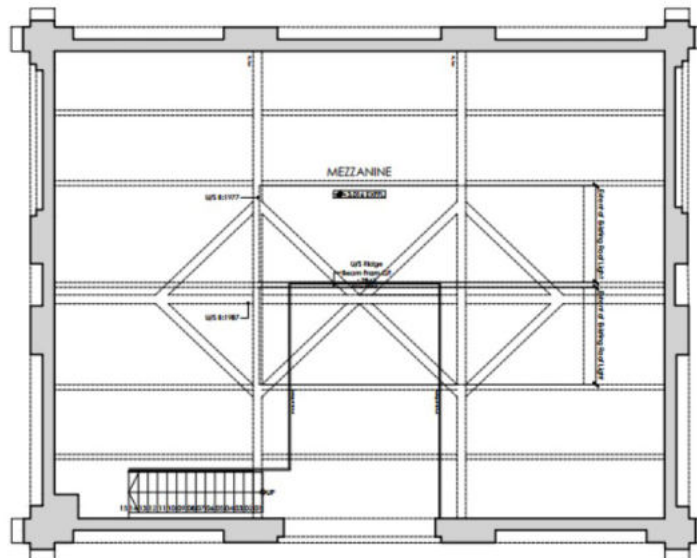


Fig 1.3 Current Mezzanine Floor Plan Showing Intricate Roof Structure

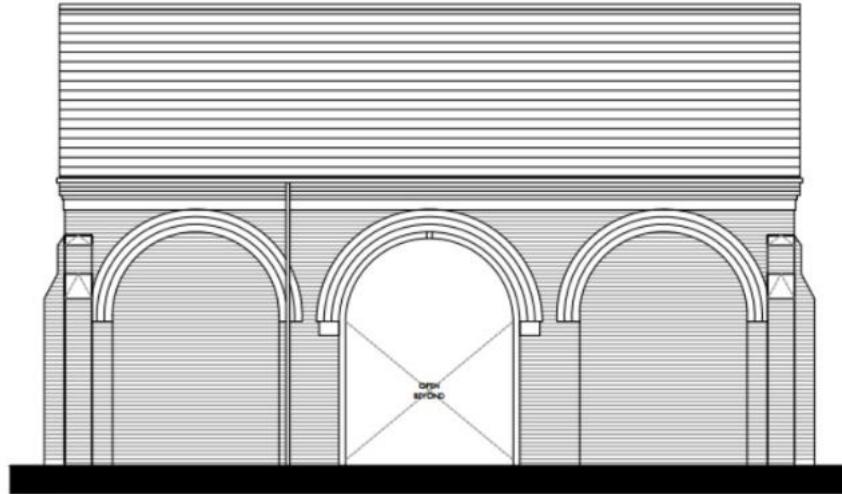


Fig 1.4 Current Existing North West Elevation showing the existing central entrance



Fig 1.5 Photo Existing North West Elevation



Fig 1.6 Photo Existing North West Elevation

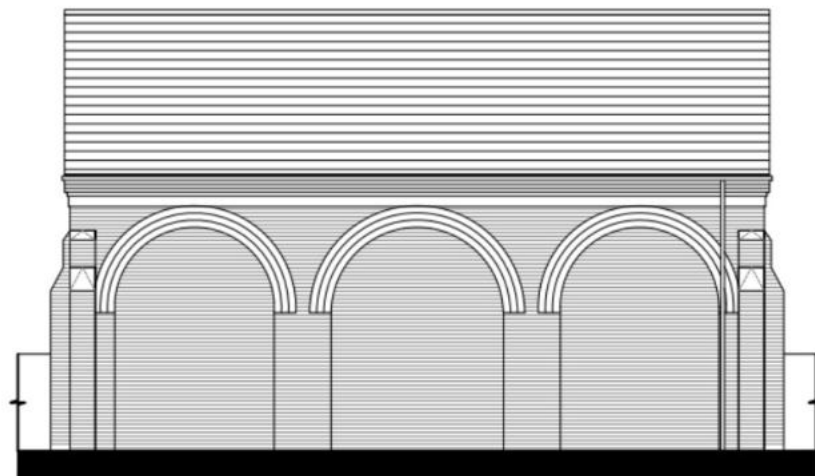


Fig 1.7 Current Existing South East Elevation Main elevation facing the railway line serving Narborough station.

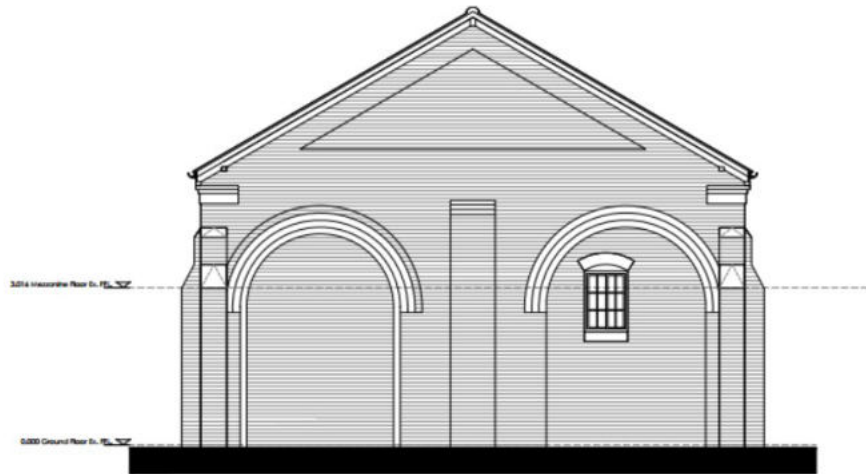


Fig 1.8 Current Existing North East Elevation Facing Main Road



Fig 1.9 Photo Existing North East Elevation

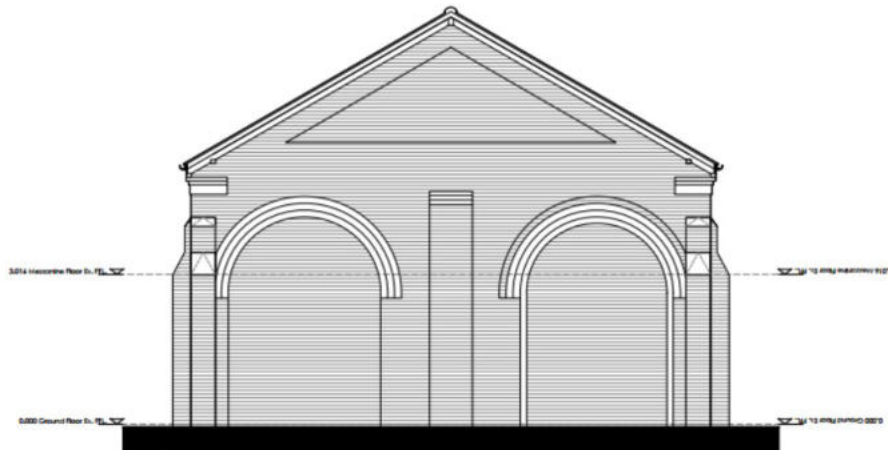


Fig 1.10 Current Existing South West Elevation Facing Existing Yard Area

2. THE AMOUNT OF DEVELOPMENT & CONTEXT

2.0 The site lies within the outer edge of the Narborough Conservation Area.



Fig 2.0 – Narborough Conservation Area (Location of the site Indicated In Red) From Blaby District planning and building control web page

- 2.1 The building is not listed by Historic England and is not a locally listed heritage asset or registered as a scheduled monument within the local authority records.
- 2.2 The surrounding area is characterised by a broad mix of commercial and retail units along with some residential properties which surround the railway stations which serves between Leicester and Birmingham
- 2.3 The site boundary line of the building is parallel to the main railway line to the south east.
- 2.4 There is no known previous planning history for this building.
- 2.5 The Environment Agency flood map records indicate that the application site is located within flood zone 1 and is therefore very unlikely to be affected by flooding from local rivers or water courses. The flood map is included within this text.

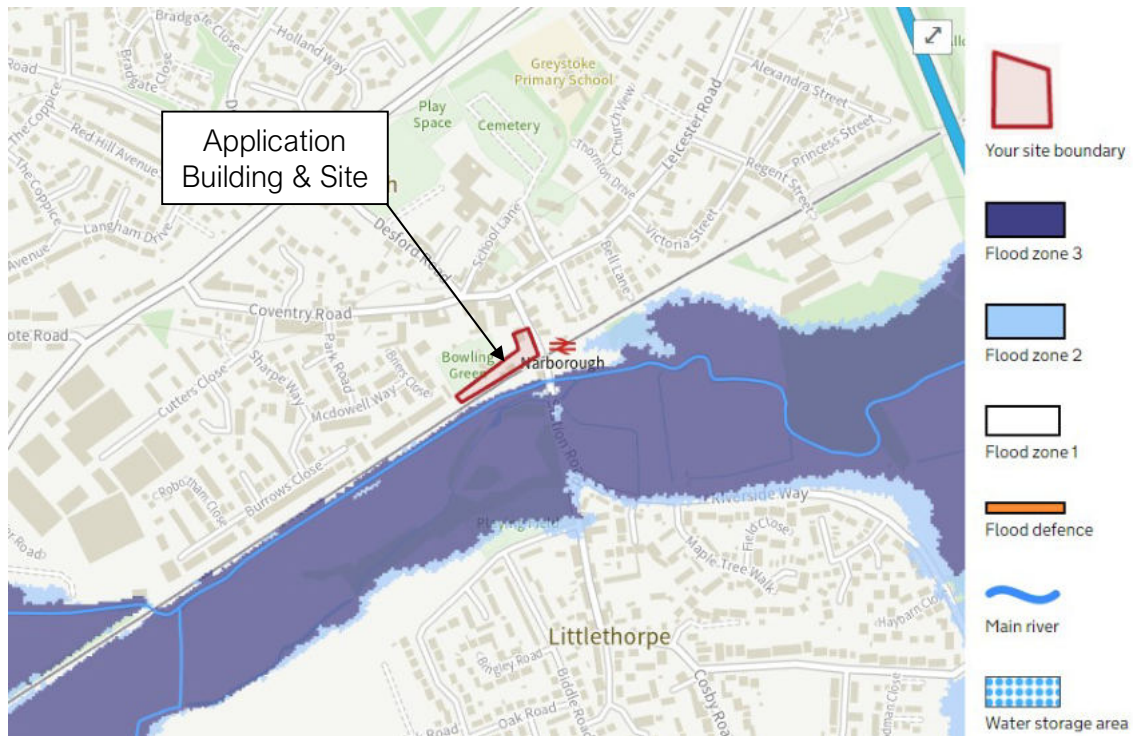


Fig 2.1 Floor Risk Map (Environment Agency Flood Map)

3. SITE ACCESS

3.0 The site is entered from an existing access from Station Road which connects Narborough to Littlethorpe. The access is a shared pedestrian and vehicular access which will be retained and un affected by the proposal.



Fig 3.0 Google Image of The Existing Site Entrance

4. THE SCALE, LAYOUT AND APPEARANCE OF THE DEVELOPMENT

- 4.01 The applicant seeks full planning approval for the change of use to offices/storage utilising the ground floor as storage and the first floor (Mezzanine) as office space.
- 4.02 The office space utilising the mezzanine floor will be accessed from the North East elevation utilising a newly formed access door and screen with a sympathetic canopy.
- 4.02 By incorporating two separate access points, one for pedestrian and vehicular access by the existing opening with large double doors to the North West Elevation this will provide safe separate access for pedestrians and vehicles/machinery
- 4.03 The access points and glazing serving the office utilise the recessed brick facade arch infill panels and respect the historic form of the former railway building. This will also minimise the impact on the building whilst maintaining the attractive North West elevation
- 4.02 The proposal is to keep the external façade as original as possible but utilise the opened up existing brick arches to form new full height glazing providing natural light to the ground and first floor office space.
The new windows will also help with visual surveillance and security of the storage yard.
The brickwork being removed to form the window openings will be put aside for future reinstatement as noted below.
- 4.03 The existing building remains very solid and is in good condition. The brickwork requires some repointing to preserve the building's longevity.
Pigeons have taken up residence in the building due to the building being open to the elements and this is beginning to effect the internal timber framing to the roof structure.
- 4.04 There is an historic internal timber decorative architectural feature at high level with exposed timber trusses rafters and the original timber sarking board to the roof. The applicant wishes to retain the existing timber features.



Fig 4.0 Existing Intricate Roof Structure

The existing building requires thermal upgrades for the change of use to comply with current building regulations which will allow the original historic timber sarking board to remain exposed as a feature. The proposal is to nominally raise the existing roof by approximately 180mm to 200mm and insulate from above to comply with the latest building regulations.

The nominal increase of the ridge height will not have an adverse effect on neighbouring properties as this is only to be three courses.

- 4.05 As part of the roof works the existing slates will be replaced with new slate to be more in keeping with the existing building's character and the local vernacular.
- 4.06 The nominal raise of the roof will be created using the existing brickwork retained from the opening up of the arches for the installation of the windows. The reuse will help to retain the building's character and aesthetics.
- 4.07 Historically, there is evidence that there was a substantial large roof light located at the ridge which the applicant wishes to reinstate at a slightly reduced size to accommodate modern construction requirements
The roof light will be key to providing additional natural light to the space, in addition to the lantern light conservation roof lights have been proposed to the South East elevation.
Due to the proximity to the railway line windows are not practical for the south eastern elevation. Below is a photo showing the original position of the rooflight



Fig 4.1 – Original Historic Rooflight location



Fig 4.2 – View of Historic Building From Mezzanine Floor

4.08 The applicant is keen to retain the original surviving materials and where required repair elements to retain the character of the building. Where damaged materials such as stone/brick details (Entrance bullnose bricks) are beyond repair, similar natural stone or brickwork matching the existing materials and provide a similar appearance to the rest of the building will be utilised.

4.09 The Old Station Yard currently has scaffolding erected around the external perimeter to three sides of the building. This has been erected to allow the applicant to replace broken and missing tiles to preserve the building structure. Below are some existing external photos of the building.



Figure 4.3 – Original Historic Side Elevation (North East Elevation)



Figure 4.4 – Original Historic Front Elevation (North West Elevation)



Figure 4.5– Original Historic Side Elevation (South West Elevation)



Figure 4.6 – View from opposite side of the Railway Line

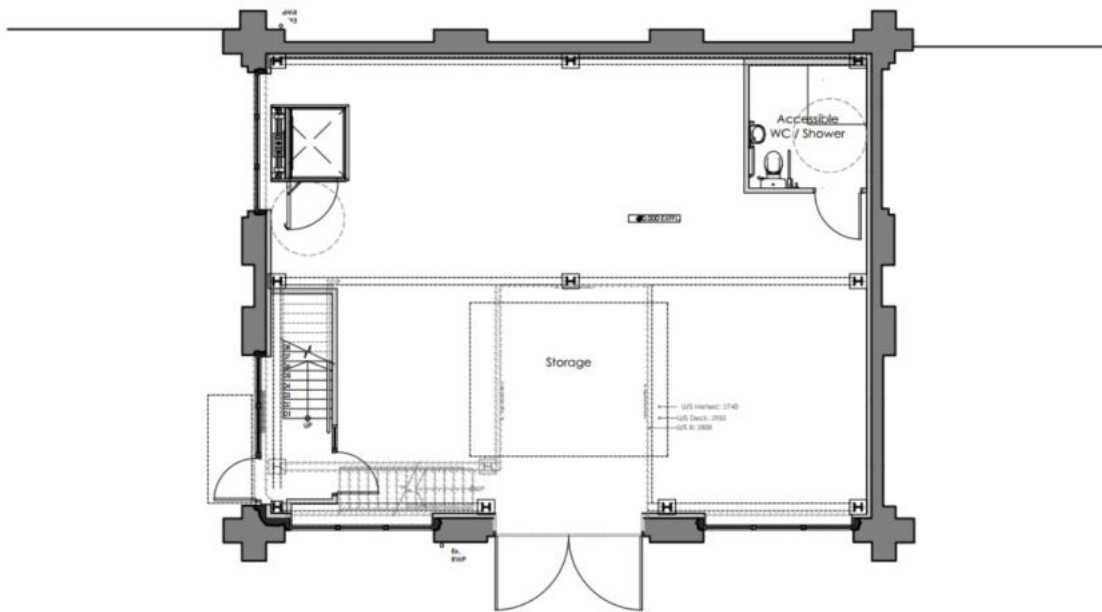


Figure 4.7 – Proposed Ground Floor Plan

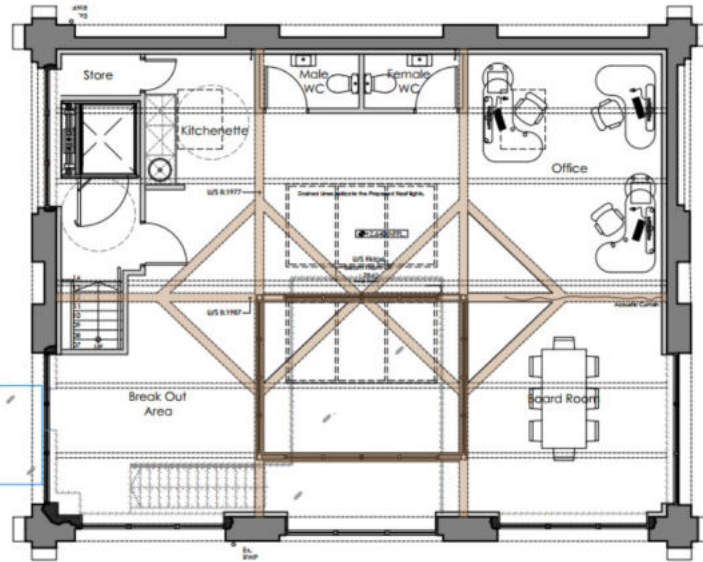


Figure 4.8 – Proposed Mezzanine Floor Plan



Fig 4.9 Proposed North West Elevation

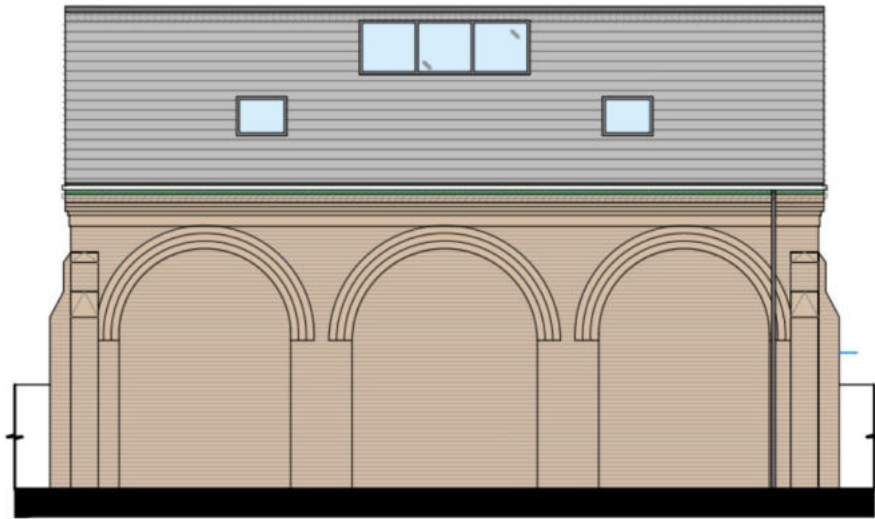


Fig 4.10 Proposed South East Elevation



Fig 4.11 Proposed North East Elevation

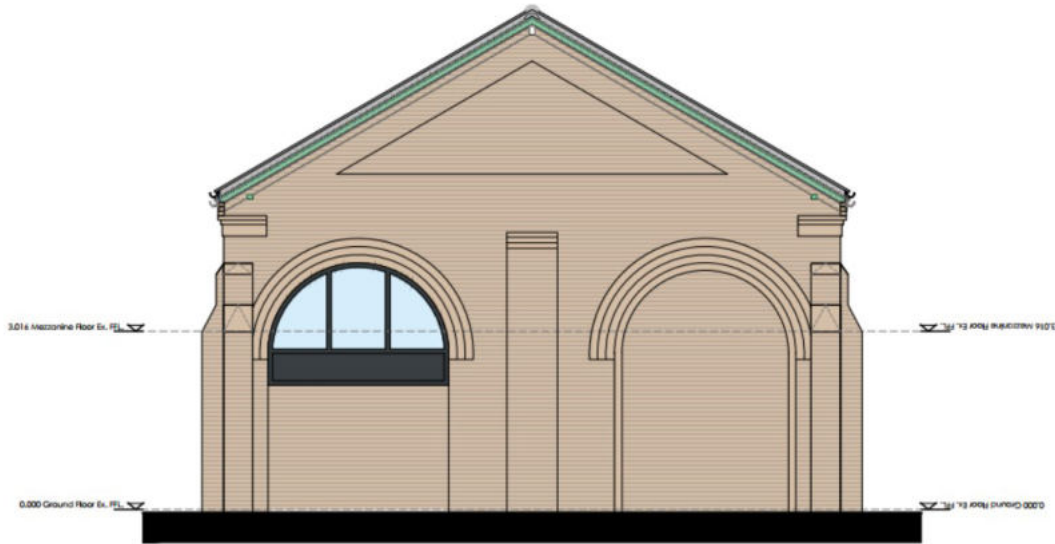


Fig 4.12 Proposed South West Elevation

- 4.10 Due to the fact that the existing building is in constant use with bright lights internally and externally. The exposed roof buildup thickness is not very deep. The building is considered to be of no beneficial ecological interest

5. LANDSCAPE OF THE DEVELOPMENT

- 5.01 The proposals do not have a great impact on the existing landscaping areas. The existing hard landscaping will provide sufficient car parking spaces for staff and visitor parking. There are already several car parking spaces provided but are currently covered by the storage of materials. These materials will be removed to provide the required parking provision and utilise the rest of the existing yard.

5.02

6. CONCLUSION

- 6.01 The development reusing this building will assist in consolidating the business and employment by creating new offices
- 6.02 The existing location is sustainable with good transport links with Narborough railway station located across the road and bus stop on Station Road. Narborough has the only passenger railway station in the district providing links to Leicester City Centre and Hinkley in less than 15 minutes. The growth of Narborough is constrained by floodplains to the south. By changing the building use will repurpose an existing building.

- 6.03 The proposal will enhance the existing buildings' use and preserve the building's longevity.
- 6.02 The proposal will reuse existing materials which will be carefully deconstructed and reused to retain the building's appearance using the original surviving materials and repairing elements where required to retain the character of the building.
- 6.03 The redevelopment of the Old Station Yard will help to preserve the historic integrity and setting of the building.
- 6.03 The proposed works will have a positive, external, aesthetic impact on the existing building and the setting within the conservation area the local vernacular.

Document Issue and Date

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(For and on Behalf of Corporate Architecture Limited)