

PLANNING, DESIGN & ACCESS STATEMENT

**164-168 Cathedral Road
Pontcanna
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Description of development:

Single storey rear extension at ground floor and covered rear lobby on the first floor. Proposed extension to rear roof dormers, external alterations including replacement windows, re-roofing, re-build host property chimney, removal of rear chimneys, internal alterations and associated works at 166-168 Cathedral Road. Removal of rear chimneys at 164 Cathedral Road.

Location:

164-168 Cathedral Road, Pontcanna, Cardiff, CF11 9JD

Date:

April 2024

Asbri Project ref:

23.263

Client:

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SITE IN REGIONAL CONTEXT



INTRODUCTION

1.1 The purpose of a Planning, Design & Access Statement (PDAS) is to provide a clear and logical document to demonstrate and explain the various facets of design and access in relation to the site and to appraise the proposed development against relevant planning policies. It also presents the details of a planning application in a way that can be read both by professionals and the public.

1.2 The diagram (left), extracted from Chapter 3 of Planning Policy Wales, summarises the five objectives of good design that should be taken into account when preparing a PDAS. The circular nature of the diagram represents the equal weightings that need to be given to each of the 5 Objectives of Good Design: Access; Movement; Character; Environmental Sustainability and Community Safety.

1.3 The submitted Design and Access Statement adheres to guidance embodied in the Welsh Government publication, Design and Access Statements in Wales, published in June 2017.

1.4 The statement is subdivided into ten sections, commencing with a brief summary of the proposals and the project brief and vision in section 2. Section 3 provides the site context and analysis whilst Section 4 comprises of a detailed Conservation Area Appraisal. Section 5 is an interpretation of the surrounding opportunities and constraints relevant to the site whilst Section 6 discusses the design development. Section 7 sets out the planning policy relevant to the proposals. Section 8 explores the proposals in detail whilst section 9 follows with an accompanying appraisal. The Statement comes to its conclusion at Section 10.





THE PROPOSAL & BRIEF AND VISION

The Proposal

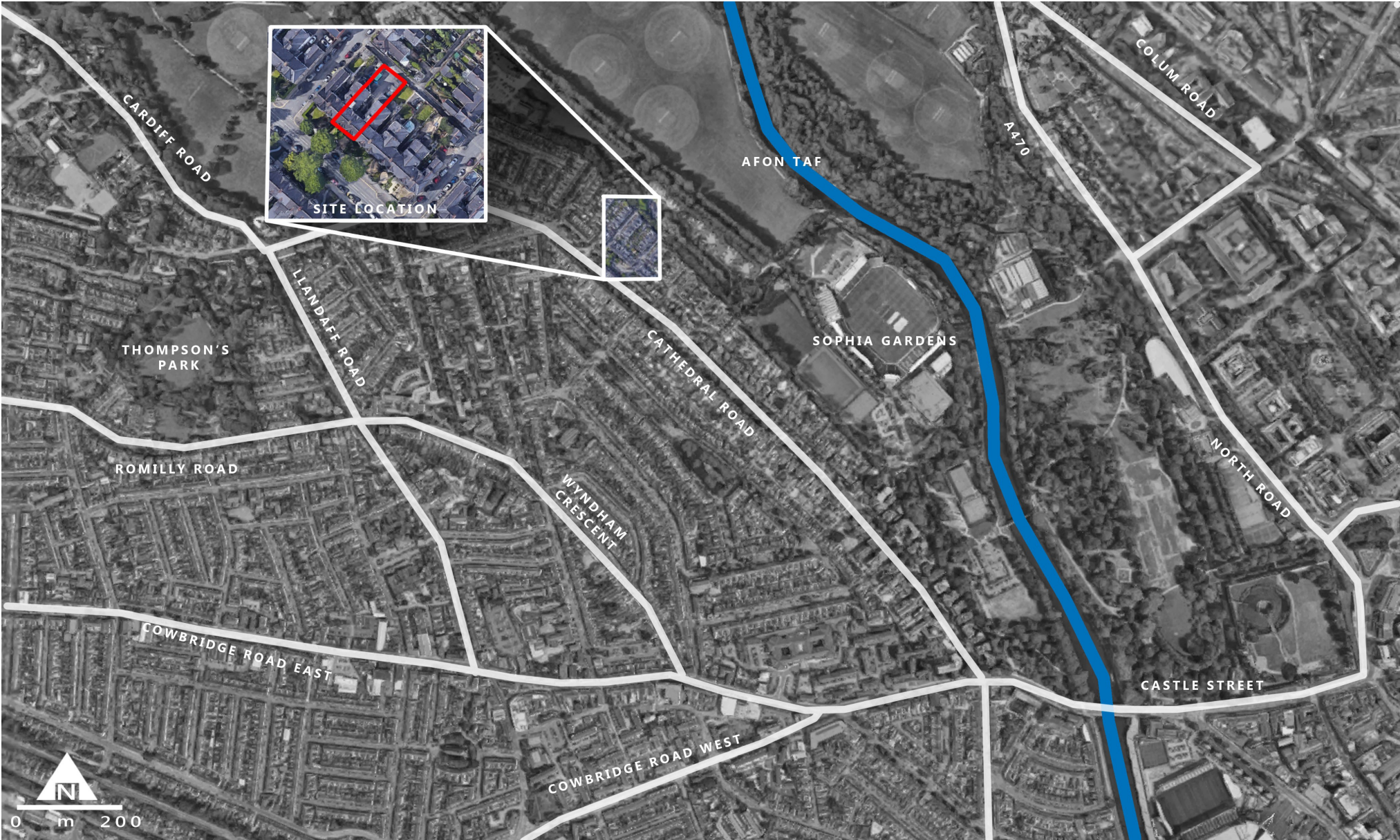
2.1 This Design and Access Statement accompanies a full application for a proposed single storey rear extension at ground floor, a covered rear lobby on the first floor, proposed extension to rear roof dormers, external alterations including replacement windows, re-roofing, re-build host property chimney, removal of rear chimneys, internal alterations and associated works at 166-168 Cathedral Road. Subject to a Party Wall Agreement, it is also sought to remove the rear chimneys at 164 Cathedral Road.

The Brief and Vision

2.2 The brief from the client was clearly set out. It is their intention to extend the existing rear roof dormers and facilitate external and internal alterations at 166-168 Cathedral Road, including the removal of the rear chimneys at 164 Cathedral Road, Cardiff.

2.3 The vision for the proposals is centered on a recognition of the site's setting within a Conservation Area, with the additions and alterations proposed considered appropriate whilst ensuring the most efficient use of available space.

SITE LOCATION



SITE CONTEXT AND ANALYSIS



Front elevation of application property



Rear elevation of application property

General Location

3.1 The main site comprises of two properties, 166-168 Cathedral Road, which have been knocked through to provide one large holding. The site is centred on the following grid coordinates: E: 316873, N: 177281. 164 Cathedral Road lies immediately to the south east of 166-168 Cathedral Road.

3.2 Cathedral Road is a busy road which serves as a connection between the city centre and the north western suburbs of the capital.

Site Features

3.3 The main site comprises of 166-168 Cathedral Road. The combining of two dwellings into one holding has resulted in the use of the ground floor as a dental clinic, with residential uses on the subsequent upper floors.

3.4 A small landscaped area lies to the front of the site, with paving, chippings and greenery seen here. To the rear, an area of hardstanding creates existing parking provision for staff and residents.

3.5 The site is level, with boundary treatments comprising of low stone walls and hedging to the front with higher stone walls and instances of fencing seen to the rear.

3.6 As defined by Natural Resources Wales' Development Advice Maps, the site lies in Zone C1 which means that the site is served by significant infrastructure, including flood defences.

3.7 The site lies within the defined Cathedral Road Conservation Area. This designation will be discussed in greater detail in Section 4 of the Planning, Design and Access Statement—Conservation Area Appraisal.

Access

3.8 An access lane runs along the site's rear boundary. Current vehicular access is established to the rear. Pedestrian access via Cathedral Road can be achieved to the south of the site.

3.9 Route 8 of the National Cycle Network (NCN) runs some 180m to the north east of the site. This route runs through the heart of Wales and is also known as Lôn Las Cymru.

3.10 The site is located approximately 45m from the nearest bus stop. The stop hosts regular services to the city centre, Cardiff's suburbs and other towns within South East Wales.

3.11 Furthermore, Cardiff Central railway station is approximately 1.6 miles to the south east of the application site. Services run from here to the wider Cardiff area, the South Wales Valleys, west and north Wales as well as services to England e.g. to Manchester Piccadilly, London Paddington, Portsmouth.

Surroundings and Residential context

3.12 Cathedral Road is contained within a defined Conservation Area and comprises of various land uses e.g. residential dwellings, public houses, clinics, guesthouses etc.

3.13 Cathedral Road is largely terraced and high in density terms. The site is adjoined on one side by 170 Cathedral Road which is operating as a dental practice (Parkside Dental Practice). On the other side, 164 Cathedral Road abuts the site and appears used for residential purposes. The removal of the rear chimneys at 164 Cathedral Road also forms part of this proposal, subject to agreement with the owner.

3.14 Cathedral Road abuts the site along its southern boundary with an unnamed rear access lane abutting to the north. This lane provides access to the rear of the properties along Cathedral Road.

3.15 Due to the site's location within a defined Conservation Area, proposals must be sensitive to its surroundings. The nearest Listed Structure is a Former Presbyterian Church located at Gileston Road. The structure is afforded Grade II listed status and is considered "a good Edwardian non-conformist church which has been very little altered." The former church lies some 165m from the application site.

3.16 There are no Scheduled Ancient Monuments within the site's immediate vicinity.

3.17 Further to the south, along Cathedral Road, lies a cluster of listed dwellings, comprising of 110-116 Cathedral Road. The four dwellings are afforded Grade II status, Cadw states their inclusion "as part of a special group of late Victorian houses in Cathedral Road. They are perhaps the best example of their type remaining in Cardiff."

EXISTING BUILT CONTEXT ALONG CATHEDRAL ROAD



SITE CONTEXT AND ANALYSIS

3.18 Further to the structures formally listed by CADW, a large section of Cathedral Road is defined as locally listed by Cardiff Council. Locally listed structures are considered of value to the city's identity and are seen as contributing to local built character and/or social and historical associations.

3.19 The surrounding built context typifies Cathedral Road, with late Victorian villas a prominent house type within the site's vicinity. Prevalent material choices comprise of grey stonework finishes with beige window surrounds.

3.20 Furthermore, the terraces are often composed as one elevation, with higher gables placed in the centre and at each end to ensure a visual emphasis.

3.21 Notable are the dwelling's relationships with the road, with a continuous building line and no outwards projection on the facades (apart from bays/porches).

3.22 Low front walls, gate piers and dividing walls are also designed to contribute positively to the street facades.

Local land uses

3.23 As discussed, Cathedral Road comprises a variety of land uses. The variety of uses ensures that the site is located in a highly sustainable location, in close proximity to services such as:

- Bus stops;
- Public houses;
- Garage;
- Coffee shops;
- Places of worship;
- Grocers;
- Bakery.

3.24 As such, the site is considered to be within a highly sustainable location.

Planning History

3.25 A review of the Cardiff Council planning history search identifies the following applications of relevance:

19/02271/MNR—Demolish/Remove steel fire escape to rear between 166/168 and 2no. Single storey lean to

extensions to rear. Construct rear single storey extensions and form a new fire escape + construct a new glazed link lobby between rear parts of 166/168 at first floor level & alter internal layout of first floor flats and construct new roof dormers to existing flats.—Granted.

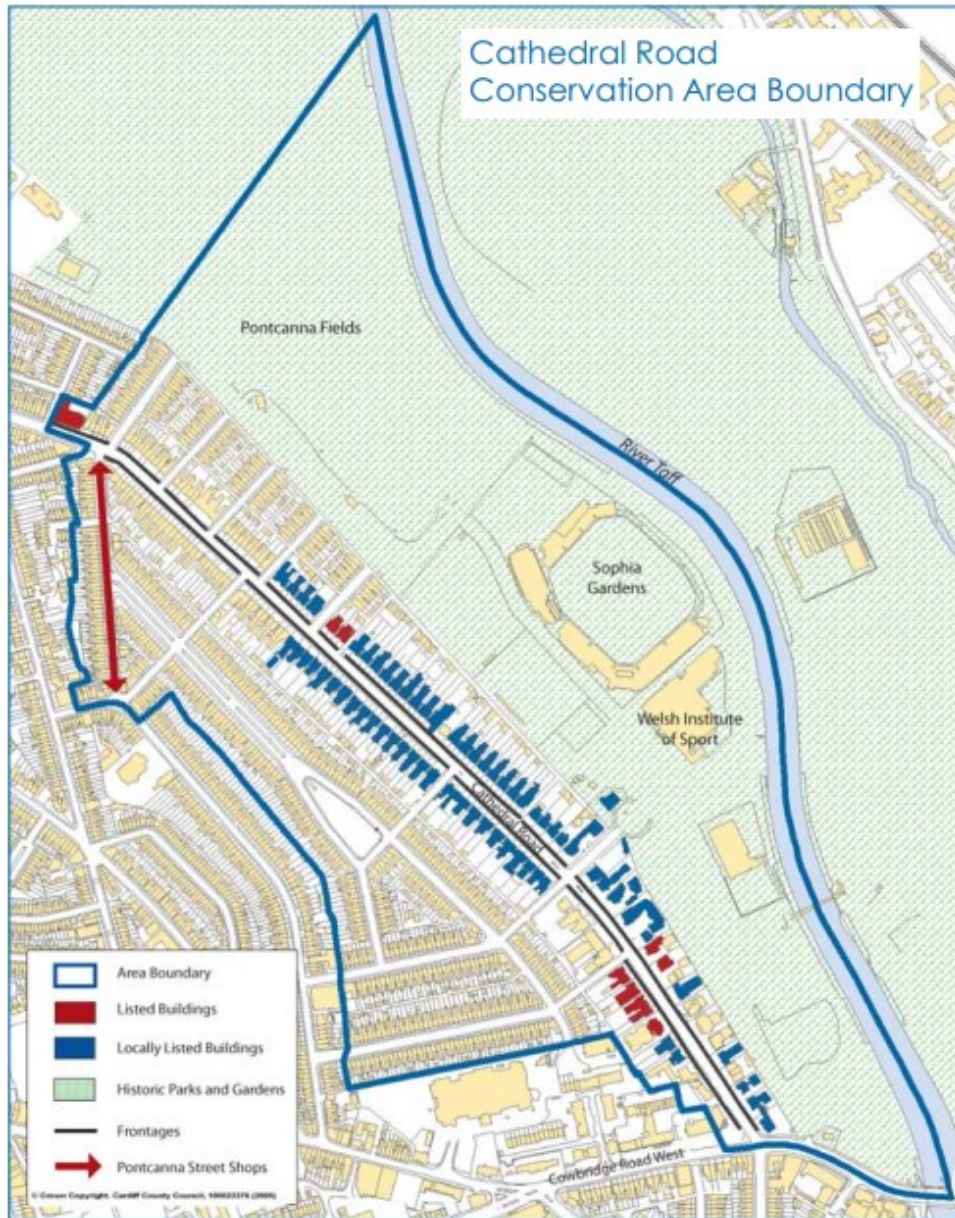
12/01564/DCI - Single storey rear extension to dental surgery with balcony over and change from pitched to flat roof to create balcony on existing single storey extension of dental surgery. Side extensions for dental surgery and dormer extensions to both rear annexes—Withdrawn.

05/00943/W— Replacement of existing and addition of two new sash windows, two dormers to rear roof—approved subject to conditions.

99/01891/W - Adjustment to existing external steel access staircase and formation of new entry door, at first floor level - Granted.

Other applications relevant to the site relate to the established use of the practice and the creation of parking provision.

CONSERVATION AREA BOUNDARY



Source: Cathedral Road Design Guide
<https://www.cardiff.gov.uk/ENG/resident/Planning/Documents/Cathedral%20Road%20Design%20Guide.pdf>

CONSERVATION AREA APPRAISAL

4.1 Cardiff has detailed information and guidance on the city's designated Conservation Areas. This appraisal discusses information made available in leaflets prepared for householders as well as the more detailed Cathedral Road Conservation Area Appraisal.

4.2 There are 27 Conservation Areas within the city with great variation in size and character. They span the city from the villages of St Fagans and St Mellons to more recent Edwardian suburbs.

4.3 Guidance prepared by the authority states that **“The designation of a Conservation Area enables everyone to understand the significance of a historic area, and helps inform the way it is maintained and planning decisions are made...many properties have a quality of architectural detailing and craftsmanship that could not be matched today, and which contribute to the unique character of an area. Designation ensures that this special character can be protected.”**

4.4 Within Conservation Areas, a high standard of design is expected. Care and sensitivity to the designation should be ensured throughout the design process. Preserving and enhancing should be driving principles within a Conservation Area.

4.5 Relevant to this application, the following guidance is relevant in terms of the following features;

- **Windows and Doors:** Original styles should be retained where possible. New windows should reflect styles and opening methods of those originally fitted. Where new windows have become necessary, their replacement shall carefully match the appearance, materials and function of the original windows. When planning permission is required, replacement of original timber sash windows with upvc windows will only be permitted where it is considered that they preserve the character of the conservation area. The size of window openings should be maintained.
- **Roofs:** Original materials should be retained and repaired. If replacement is planned the scope for the re-use of the original roofing material should be assessed as roofing works proceed. Reclaimed or new natural slates and tiles are the best way to

preserve the special interest of the building and wider conservation area. However, good quality replica slates are sometimes an acceptable alternative covering for an unlisted building in the conservation area.

- **Dormers and roof lights:** New dormer windows and roof lights should not be visible from the street unless they are of sympathetic design. Dormer windows and rooflights are therefore more likely to be considered acceptable on rear elevations as they tend to be discrete locations. However where a rear elevation of a building is very prominent from the highway (excluding the rear service lanes) they are unlikely to be acceptable;
- **Chimneys:** The removal of a chimney which makes a contribution to the conservation area will not normally be supported; and
- **Refuse and Waste Management:** It is important that development proposals involving waste management are sympathetic to the historic built environment.

4.6 The Cathedral Road Conservation Area Appraisal goes into greater detail on the area itself. The Cathedral Road area was developed in the late 19th century by the 3rd Marquis of Bute. Cathedral Road was designed as a grand avenue with a distinctive quality of design.

4.7 With specific reference to the area within which the site lies (from Sophia Close north-westwards), some current issues are raised e.g. increased pressure for parking/servicing, pressure for extending into rear gardens, heavy traffic throughout the day etc.

4.8 Opportunities for the area are proposed in the form of signage removal or rationalisation, enhancement and restoration of existing traditional boundary treatments, enhanced links to nearby parks and fields and removal of unsightly additions such as fire escapes etc.

4.9 Other more general opportunities are presented in the way of; maintain the special character of the existing Cathedral Road Conservation Area, ensure new developments are well integrated, increase understanding and appreciation of the area's character, encourage its protection and support local initiatives to keep the area attractive and lively.

4.10 Proposals are considered to adhere to the guidance provided which highlights that the **“most effective way to enhance the Conservation Area is to ensure that those planning work or development understand its character.”**

OPPORTUNITIES AND CONSTRAINTS PLAN



INTERPRETATION

Interpretation Opportunities and Constraints

5.1 The concept for the development of the site has derived from the following;

- Full site analysis including a full desktop study of the site and its surroundings; and
- Discussions with the client and a full understanding of the brief and vision of the project.

5.2 The above steps have presented the key opportunities and constraints for the site, which are outlined below;

Opportunities

Current use and location – the site is already occupied by both residential apartments and an operating dentist clinic, lying comfortably within the defined Settlement Boundary. It is considered that proposals are in line with its established use and location.

Existing Residential and Business Context – the site is located adjacent to a high amount of residential development and business uses therefore proposals are considered a conforming land use in this instance.

Proximity to local services – the site is well-served in terms of local services and facilities such as local shops, public houses, worship facilities, public transport and public open spaces.

Connectivity – As discussed, the site is well placed in terms of connectivity, located in the Central Area. Proximity to public transport and sustainable travel nodes strengthen the site's sustainability credentials.

Constraints

Conservation Area – As aforementioned, the site is located within a defined Conservation Area. Due regard must be given to the surrounding context and character in order to ensure no negative impacts as a result of the development.

Existing built context – The site is surrounded by existing residential properties and businesses, therefore consideration must be given to the amenity of these dwellings.

PRECEDENT STUDIES WITHIN SURROUNDING CONTEXT



Rooflights located at the rear of neighbouring properties along Cathedral Road. Source: Google Street View.



Rear roof dormers located at the rear of neighbouring properties along Cathedral Road. Source: Google Street View.

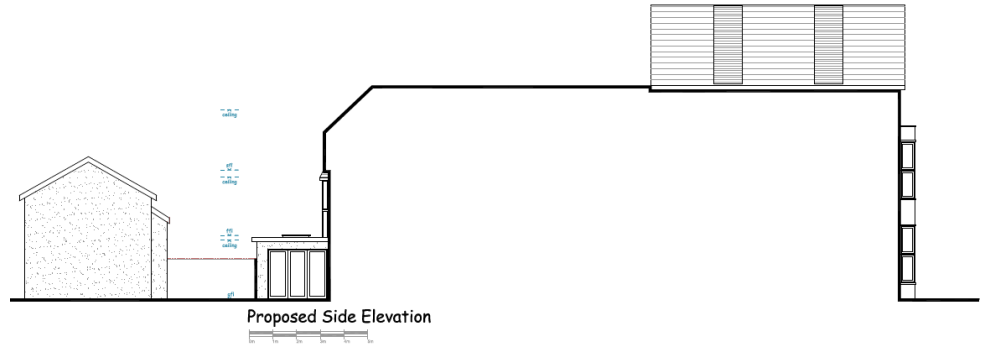


Proposed Rear Elevation

Proposed Front Elevation



Proposed Side Elevation



Proposed Side Elevation

Proposed Elevations for application ref. 21/01463/MNR (126 Cathedral Road)

DESIGN DEVELOPMENT

6.1 In coming to an acceptably designed proposal, due consideration was given to the character of the surrounding area and the appearance and design of properties within the vicinity. In this respect, close attention was paid to the Cathedral Road Conservation Area appraisal which is referenced and discussed in further detail above in this document.

6.2 Further to the description of the Conservation Area already discussed, the surrounding area comprises of large Victorian terraced dwellings with small landscaped or paved front gardens bounded by low boundary treatments to the front. Large front bay windows and regularly placed chimneys characterize these Victorian properties, when viewed from Cathedral Road. Direct vehicular access to properties located along this stretch of Cathedral Road is via an unnamed rear lane. The rear of these properties accommodate residential gardens as well as car parking, namely for commercial elements. A number of alterations and additions are visible to the rear of these properties, including projecting first floor bay windows, fire escapes, roof lights, single storey extensions, conservatories, as well as dormer roof extensions.

6.3 Notwithstanding the above, the proposal has also been developed with consideration of the most recent planning permission at the site; application reference 19/02271/MNR, referenced above. This permission was granted 27th May 2020 and as such, the permission remains live and could be implemented until 27th May 2025. This forms a fall back position to which any forthcoming assessment of this new proposal should consider.

6.4 Initial pre-app discussions were held in relation to the proposal subject of application ref. 19/02271/MNR. The proposal subsequently developed from this initial pre-application response. The proposal sought to:

- Remove the steel fire escape to the rear between 166 and 168 Cathedral Road;
- Demolish 2no. Single storey lean to extensions to the rear;
- Construct rear single storey extensions;

- Form a new fire escape;
- Construct a new glazed link lobby between the rear parts of 166-168 Cathedral Road at first floor level;
- Alter internal layout of first floor flats; and
- Extend rear roof dormers to existing flats.

6.5 The officer report stated that the proposed extensions and alterations were considered acceptable in regard to scale and design and would not prejudice the character and appearance of the Conservation Area. Their position to the rear of the property aided in minimizing their impact on the character of the area. The Officer acknowledged that a number of properties in the area have also been extended and altered, including the provision of rear dormers. In terms of residential amenity, the proposals were not considered overbearing or un-neighbourly and subject to conditions, would not prejudice the privacy of neighbours (subject to obscuring windows). The rear courtyard provided sufficient space to accommodate the required number of off-road car parking spaces for the property. Permission was granted subject to a total of 9 conditions.

6.6 Whilst the permission is still live and could be implemented in full, the applicant now wishes to further extend the rear roof dormers to facilitate internal re-configurations that would allow larger apartments, albeit no greater number of apartments are proposed. The covered lobby as initially approved under application ref.19/02271/MNR is again proposed as part of this new proposal, although its acceptability was established by this permission. The applicant also proposes to infill the rear courtyard space initially approved under the original permission, to form the proposed rear single storey extension. Further additional external alterations are also sought, which are explored further in the proposal and appraisal section below.

6.7 The design of the proposal as submitted is considered to be in keeping with the character of the surrounding area, as is discussed further in the appraisal section below. Rear roof dormers are common additions to properties along Cathedral

Road, as are rear roof lights. Some examples are presented, *left*.

6.8 A recently approved scheme (ref. 21/01463/MNR) at 126 Cathedral Road, some 165m south east of the application site, reflects several elements of the proposed scheme at 164-168 Cathedral Road. The proposal at no. 126 included the *“Change of use of existing guest house to include 8no apartments, including single storey rear and dormer roof extensions and conversion of existing outbuilding”*. The supporting proposed elevational plans for this scheme are shown, *left*.

6.9 It is clear that rear roof dormers are common within the vicinity and the previous permission at the application site (ref. 19/02271/MNR) establishes the acceptability of such dormers in this location. The dormers as approved at 126 Cathedral Road would facilitate further apartments. This is also the intention at the application site by means of the new proposed dormers subject of this planning application.

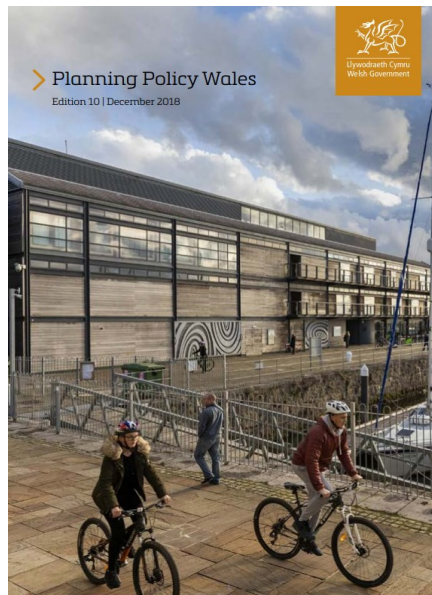
6.10 The officer report for the scheme at 126 Cathedral Road states that the proposed dormer is set within the side roof space somewhat screened by existing built form and, therefore, not prominent. It also states that a number of similar features exist to the rear of neighbouring properties. In terms of the amenity of future occupiers, the officer states that the level of internal accommodation for each proposed dwelling is of an adequate size and layout.

6.11 It is found that the removal of rear chimneys on properties located within the Conservation Areas of Cardiff to be acceptable. For example, application ref. 19/01291/DCH permitted the removal of the chimney stack at the rear of 10 and 12 Talbot Street. The officer report states that the rear chimney stack is not prominent from any public views due to its location in the middle of a terrace and at the rear, and it is also not decorative. As such, it was concluded that its removal was regarded as preserving the character of the area. This stance is also deemed applicable to the proposals at 164-168 Cathedral Road.

PLANNING POLICY



PLANNING POLICY



Introduction

7.1 The planning policy framework for the determination of this application is provided by the content and scope of National Planning Policy, which is contained within Future Wales, the eleventh edition of Planning Policy Wales (PPW) and its associated Technical Advice Notes (TANs), together with the Local Planning Policy and its supplementary planning guidance.

National Level Future Wales

7.2 The National Development Framework: Future Wales – the National Plan 2040 was published on 24th February 2021. ‘Future Wales’ sets out the Welsh Government’s strategy for addressing key national priorities through the planning system, including sustaining and developing a vibrant economy; achieving decarbonisation and climate-resilience; developing strong ecosystems; and improving the health and well-being of our communities.

7.3 Future Wales’ spatial strategy is a guiding framework for where large scale change and nationally important developments will be focused over the next 20 years. The strategy blends the existing settlement patterns and the distribution of jobs and homes with a vision of

7.4 Future Wales divides Wales into four regions, North, Mid Wales, The South West and The South East. The application site lies within the South East region which comprises Blaenau Gwent, Bridgend, Caerphilly, Cardiff, Merthyr Tydfil, Monmouthshire, Newport, Rhondda Cynon Taf, Torfaen and the Vale of Glamorgan and is the most populous region of Wales, with over 1.5 million residents.

7.5 Policy 1 (Where Wales will grow) confirms that whilst the Welsh Government supports sustainable growth in all parts of Wales, in three National Growth Areas there will be growth in employment and housing opportunities and investment in infrastructure. These comprise;

- **Cardiff, Newport and the Valleys;**
- Swansea Bay and Llanelli; and
- Wrexham and Deeside.

7.6 Policy 2 (Shaping Urban Growth and Regeneration) confirms that “the growth and regeneration of towns and cities should positively contribute towards building

sustainable places that support active and healthy lives, with urban neighbourhoods that are compact and walkable, organised around mixed-use centres and public transport, and integrated with green infrastructure.

7.7 Policy 33 confirms that “*Cardiff, Newport and the Valleys will be the main focus for growth and investment in the South East region.*” In relation to Cardiff in particular, it states that “*The Welsh Government support’s Cardiff’s status as an internationally competitive city and a core city on the UK stage.*”

Planning Policy Wales

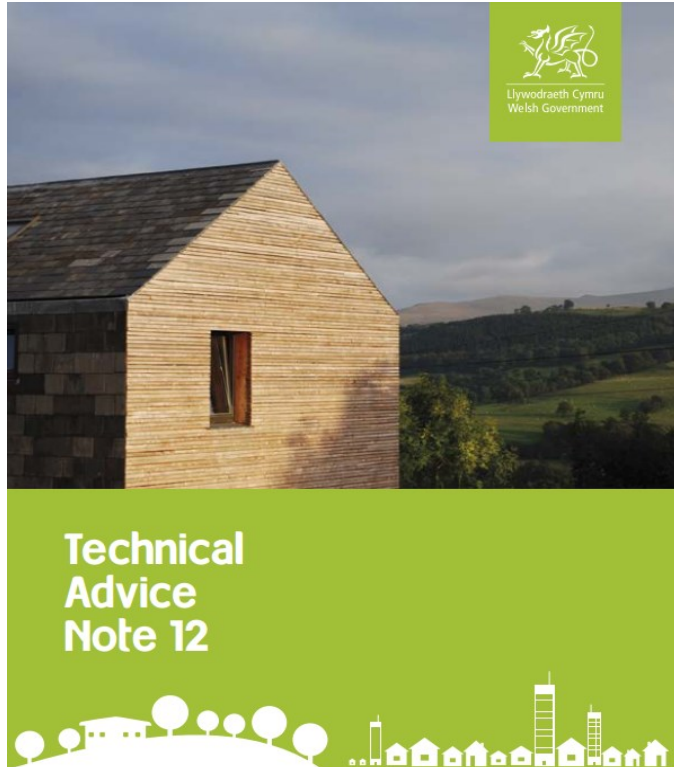
7.8 National planning policy is contained within the 11th edition of Planning Policy Wales (PPW), published by the Welsh Government in February 2021. PPW is supported by 21 topic-based Technical Advice Notes (TANs), which are also relevant. PPW is the Welsh Government’s principal planning policy document, setting out the context for sustainable land use planning policy, within which Development Plans are prepared and development.

7.9 The primary objective of PPW is to ensure that the planning system contributes towards the delivery of sustainable development and improves the social, economic, environmental and cultural well-being of Wales, as required by the Planning (Wales) Act 2015, the Well-being of Future Generations (Wales) Act 2015 and other key legislation. PPW and the National Development Framework (NDF) set out how the planning system at a national, regional and local level can assist in delivering these requirements through Strategic Development Plans (SDPs) and Local Development Plans (LDPs).

7.10 Sustainable Development is defined at Page 7 of PPW as follows: “*the process of improving the economic, social, environmental and cultural well-being of Wales by taking action, in accordance with the sustainable development principle, aimed at achieving the well-being goals.*”

7.11 Section 3.17 of PPW relates to Design and Access Statements (DAS) discussing that a DAS should communicate what development is proposed, demonstrate the design process that has been undertaken and explains how the objectives of good design and placemaking have been considered from the outset of the development process.

PLANNING POLICY



Design
March 2016



Policy	Relating to
Strategic Policies	
KP1	Level of Growth
KP3 (B)	Settlement Boundaries
KP5	Good Quality and Sustainable Design
KP8	Sustainable Transport
KP16	Green Infrastructure
KP17	Built Heritage
Detailed Policies	
H5	Sub-division or conversion of residential properties
EN4	River Corridors
EN9	Conservation of the Historic Environment
T1	Walking and Cycling
T2	Strategic Rapid Transit and Bus Corridors
W2	Provision for Waste Management Facilities in Development

PLANNING, DESIGN & ACCESS STATEMENT

PLANNING POLICY

7.12 Placemaking is a key thread running through the 11th Edition of PPW. It is described as an **“inclusive process, involving all of those with a professional or personal interest in the built and natural environment, which focuses on developing plans, making decisions and delivering developments which contribute to the creation and enhancement of sustainable places.”**

7.13 With reference to design, paragraph 3.5 states that **“Good design is inclusive design”**. Furthermore, proposals are expected to **“place people at the heart of the design process”**.

7.14 Para 3.55 discusses the Welsh Government’s preference for the utilisation of previously developed land for development, wherever possible. It states; **“such land should generally be considered suitable for appropriate development where it’s re-use will promote sustainability principles and any constraints can be overcome.”**

7.15 With reference to transport, Section 5.3.1 emphasises how the planning system should **“facilitate the delivery, decarbonisation and improvement of transport infrastructure in a way which reduces the need to travel, particularly by private vehicles, and facilitates and increases the use of active and sustainable transport.”**

7.16 In line with the Sustainable Transport Hierarchy outlined in Figure 8 of Planning Policy Wales (PPW), the use of private motor vehicles is clearly discouraged. Paragraph 4.1.13 of PPW further outlines the purpose of the hierarchy as a tool that **“should be used to reduce the need to travel, prevent car-dependent developments in unsustainable locations, and support the delivery of schemes located, designed and supported by infrastructure which prioritises access and movement by active and sustainable transport.”**

7.17 The importance of green infrastructure is highlighted throughout PPW. The updated Chapter 6 of PPW (Distinctive and Natural Places) is duly noted, which came into immediate effect when it was published in October 2023. Section 6.2.5 has been updated to require the submission of a green infrastructure statement with all planning applications. It states, **“This will be proportionate to the scale and nature of the development proposed and will describe how green infrastructure has been incorporated into the proposal. In the**

case of minor development this will be a short description and should not be an onerous requirement for applicants. The green infrastructure statement will be an effective way of demonstrating positive multi-functional outcomes which are appropriate to the site in question and must be used for demonstrating how the step-wise approach (Paragraph 6.4.21) has been applied.” Furthermore, sections 6.4.2, 6.4.5 and 6.4.21 have been updated to strengthen reference and clarification regarding the requirement to maintain, enhance and deliver a net benefit for biodiversity and ecosystem resilience.

Technical Advice Notes

7.18 This application has been prepared in deference to the latest version of TAN 12: Design, which was updated in October 2017 and the suitability of the proposals in this design context are demonstrated in this Design and Access Statement.

7.19 Principles set out within TAN 24: The Historic Environment are also incorporated in the assessment of the proposal. It states that **“...policies on the historic environment and the need to include locally specific policies for the conservation and enhancement of historic assets if appropriate. National and local policies provide an important part of the framework for the consideration of individual proposals for development and help guide developers in preparing planning applications.”**

Local Level

Cardiff Council Local Development Plan

7.20 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that planning application decisions should be made in accordance with the Authority’s adopted development plan, unless material considerations indicate otherwise. In this instance, the statutory development plan for this application site is provided in the Cardiff Local Development Plan (2006-2026). Relevant planning policies are contained in the table to the left. Further detail of these relevant policies are explored below.

KP1: Level of Growth

7.21 Policy KP1 outlines the Plan’s aim to provide new jobs as well as new dwellings, contributing to housing delivery in

the City.

KP3(B): Settlement Boundaries

7.22 There is general support for development proposed within Cardiff’s settlement boundaries as shown on the Proposals Map, subject to the proposal’s compliance with material planning considerations. The policy imposes a strict control on development located in the countryside, outside of the settlement boundaries.

KP5: Good Quality and Sustainable Design

7.23 This policy highlights that all new development will be required to be of a high quality, sustainable design and make a positive contribution to the creation of distinctive communities, places and spaces. Development should, amongst other things, respond to the local character and context of the built and landscape setting so that layout, scale, form, massing, height, density, colour, materials, detailing and impact on the built and natural heritage area all addressed. Development must not cause undue effect on the amenity of neighbouring occupiers and should connect positively to surrounding communities.

KP8: Sustainable Transport

7.24 Policy KP8 emphasises the need for development to be well integrated with the transport network to make developments accessible by sustainable travel modes and achieve a necessary shift away from car-based travel.

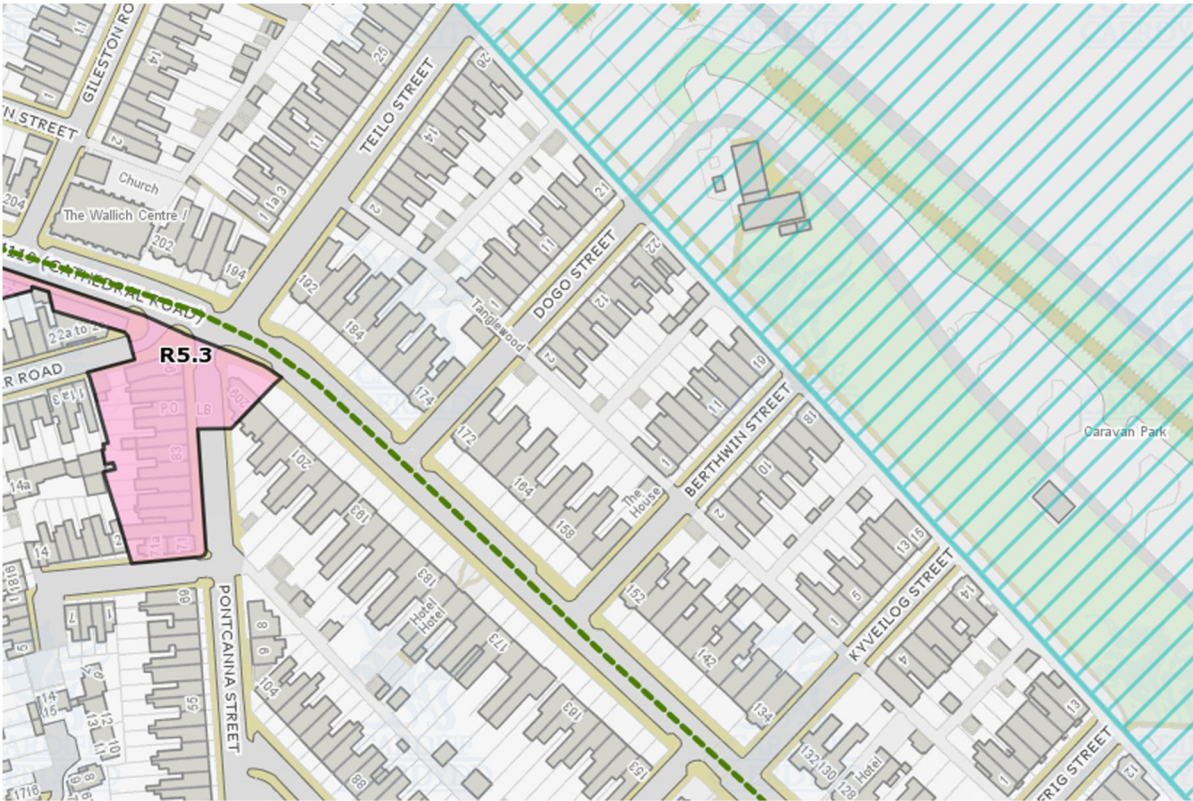
KP16: Green Infrastructure

7.25 Notwithstanding the most recent update to PPW in relation to Green Infrastructure, Policy KP16 states that proposed development should demonstrate how green infrastructure has been considered and integrated into the proposals. If development results in overall loss of green infrastructure, appropriate compensation will be required.

KP17: Built Heritage

7.26 Policy KP17 highlights the importance of protecting, managing and enhancing Cardiff’s distinctive heritage assets, as required by legislation and PPW. The Policy affords appropriate protection to Conservation Areas and advises that it should be rear alongside the adopted Conservation Area Appraisal prepared for the relevant area.

LDP PROPOSAL'S MAP



PLANNING POLICY

H5: Sub-division or Conversion of Residential Properties

7.27 Proposals for any conversion to, or sub-division of, a residential building will only be permitted where it meets the four criteria set out in Policy H5. The following four criteria must be met:

- i. The property is of a size whereby the layout, room sizes, range of facilities and external amenity space of the resulting property would ensure an adequate standard of residential amenity for future occupiers.
- ii. There would be no material harm to the amenity of existing, nearby residents by virtue of general disturbance, noise or overlooking.
- iii. The cumulative impact of such conversions will not adversely affect the amenity and/or the character of the area.
- iv. Does not have an adverse effect on local parking provision.

EN4: River Corridors

7.28 This Policy seeks to protect, promote and enhance Cardiff's river corridors. They include the rivers together with adjoining open space and other predominantly open land that together form the strategically important corridor of mixed open spaces that run through the heart of the urban area. The site does not lie directly within a river corridor. It does, however, lie in close proximity to a river corridor as indicated on the LDP Proposal's Map to the left.

EN9: Conservation of the Historic Environment

7.29 Policy EN9 seeks to ensure that all new developments within historic areas with heritage value should be designed in such a way as to preserve or enhance their special character, and any assets' architectural quality, historic and cultural significance, character, integrity and/or setting.

T1: Walking and Cycling

7.30 Policy T1 supports developments which incorporate design features and facilities that encourage everyday journeys to be made by foot or bicycle, as opposed to travel by car. This includes development that supports facilities including secure cycle parking, and the provision of Car-Free Zones.

T2: Strategic Rapid Transit and Bus Corridors

7.31 A strategic bus corridor enhancement route runs along Cathedral Road, as shown, *left*. Policy T2 seeks to make provision for improvements to maximise the efficiency and attractiveness of bus services along these routes through reducing journey times and improving journey time reliability.

W2: Provision for Waste Management Facilities in Development

7.32 New developments must ensure adequate provision is made for waste management facilities, in order to meet waste recycling targets set by European and national targets. provision for storage and recycling facilities and other management of waste must also be secured. Policy W2 looks to secure this within new developments.

Supplementary Planning Guidance (SPGs)

7.33 The following SPG's and other Supporting documents are considered relevant to this proposal:

- Managing Transportation Impacts—Incorporating Parking Standards (July 2018);
- Residential Extensions and Alterations (November 2017);
- Waste Collection and Storage Facilities (October 2016);
- Residential Design Guide (January 2017); and
- Cathedral Road Conservation Area (An advisory leaflet for householders) Updated March 2010
- Conservation Areas General Information (An advisory leaflet for householders) Updated July 2007;
- Cathedral Road Conservation Area Appraisal 2007.

EXISTING ELEVATIONAL PLANS



EXISTING FRONT ELEVATION - 1:100



EXISTING REAR ELEVATION - 1:100

THE PROPOSAL

Overview

8.1 This chapter explores the relevant design and access facets associated with the application and will provide a description of development against the five objectives of good design; a set of principle considerations, as outlined in Technical Advice Note 12, to ensure developments effectively respond to local context so that they assimilate into the locality and are functional for their intended user.

8.2 The Five Objectives of Good Design are Access, Movement, Character, Environmental Sustainability and Community Safety – each of which will be dealt with separately in turn below.

Access and Movement

8.3 Vehicular access into the site is proposed to the rear, as per existing arrangements. An access lane runs along the rear of the site with parking provision in place.

8.4 Pedestrian access can be achieved to the rear and to the front. Clinical staff can use both front and rear entrances whilst patients will use the front entrance at 166 Cathedral Road which leads to the waiting rooms. Residents can access their apartments by using the front (at 168 Cathedral Road) and rear doors. This access arrangement is established given the most recent planning permission (ref. 19/02271/MNR).

8.5 There are no lifts within the building, with access to the apartments via stairways.

8.6 Existing arrangements for car parking (i.e. located to the rear) will be retained within the proposals. The spaces will ensure that drivers are able to reverse in and drive forward out of the space. It is acknowledged that the site sits in the Central Area in terms of parking and therefore there is no requirement for resident parking provision in this location.

8.7 Cycle space provision is located to the rear of the property, as well as to the front. A secure and covered bike shed/store will be provided; one to the rear, and one to the front. This will accommodate both the residential and commercial uses.

8.8 As discussed in Section 3, the site benefits from being in a highly sustainable location. The site lies some 100 metres from the defined Local Centre at Pontcanna Street. Here, a

Co-Op convenience store, cafes, restaurants and a launderette ensures an abundance of local services and facilities.

8.9 As aforementioned, National Cycle Route 8 of the National Cycle Network (NCN) runs some 180m to the north east of the site. This route runs through the heart of Wales and is also known as Lôn Las Cymru. This further demonstrates the site's accessibility, with particular reference to sustainable travel modes.

8.10 As depicted on the Local Development Plan Proposals Map (Page 18), a strategic bus corridor runs along Cathedral Road. A bus stop is located approximately 45m from the nearest bus stop. The stop hosts regular services to the city centre, Cardiff's suburbs and other towns within South East Wales. The following services run from the stop closest to the site;

- 24 City Centre—Llandaff North via Whitchurch;
- 63/63A City Centre—Radyr and Morganstown via Llandaff, Danescourt
- 122 Tonypandy—Cardiff;
- 124 Cardiff—Maerdy; and
- 320 Cardiff—Talbot Green.

Character

Amount, Scale and Layout

8.11 The proposals relate to extended rear roof dormers as well as external works to the building and internal alterations. It is proposed to internally reconfigure the existing flats at 166-168 Cathedral Road and provide 4no larger flats over the first and second floors, which is facilitated through the extended dormers. There would remain a total of 4no flats as per the existing arrangement, with the first and second floors to remain in residential use. The covered lobby at first floor is as approved under permission 19/02271/MNR. Whilst the single storey rear extension does involve the infill of the previously approved rear courtyard space, it would not project any deeper than the extension originally approved.

8.12 Whilst it is acknowledged that the proposal involves an increase in bedrooms and that this may increase footfall

levels, the site's sustainability credentials are reiterated. With ample public transport provision, proximity to local services and its location within the central zone for parking, it is not considered that the scale of the proposals will impact negatively in any way.

8.13 Proposals accord with the existing built context; a clinic at ground floor with residential use above is common along Cathedral Road. It is considered that the proposal demonstrates a sensitive and reasonable approach to providing larger residential dwellings across two floors which would be in keeping with the character of the surrounding area and would contribute to the city's housing delivery in a highly sustainable location.

8.14 As discussed, formalised parking provision is to the rear of 166-168, as per existing arrangements. Cycle parking will also be provided to the rear, along with bin storage. Bin stores located to the rear will ensure they do not appear visible from public vantage points, in particular from Cathedral Road which strongly contributes to the character of the Conservation Area. To the front, the curtilage will comprise of a formal landscaped garden carefully designed to compliment the character of the area, ensuring it strongly contributes to the Conservation Area.

8.15 The site is located within a terrace and is abutted by existing structures to the east and west. The site fronts onto Cathedral Road and opposite properties, however, this relationship is considered acceptable due to it being long-established and across a highway. It is considered that the back to side relationship with existing properties at Dogo Street is acceptable and design measures and sufficient separation distances ensure that there is no adverse overlooking.

8.16 The site is considered to be broadly level.

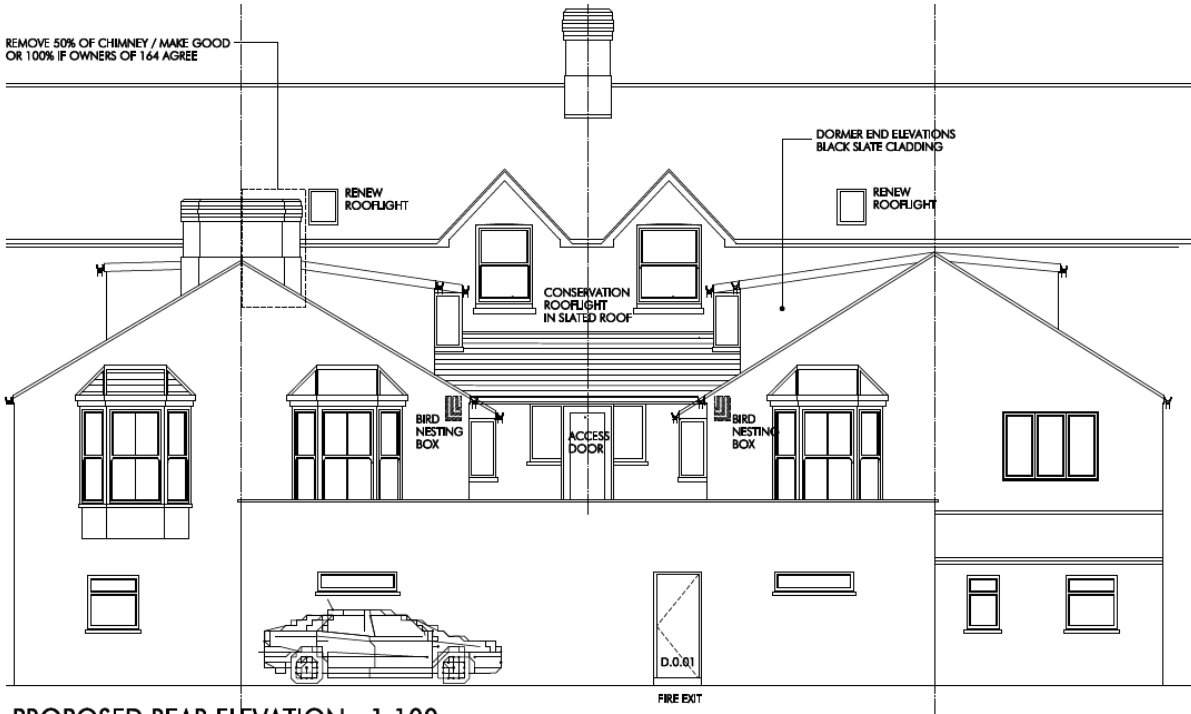
8.17 Proposals are confined within the existing footprint of 164-168 Cathedral Road. Internal alterations are proposed to both the first and second floor, which are to be facilitated by virtue of the extended rear roof dormers.

8.18 The floorspace for each flat is contained within the accompanying document referenced 'Floor Space Calculations'. This demonstrates that each flat is compliant with the space standards set out within Cardiff Council's Flat Conversions SPG. This, along with other measures and design considerations

PROPOSED FRONT AND REAR ELEVATIONAL PLANS



PROPOSED FRONT ELEVATION - 1:100



PROPOSED REAR ELEVATION - 1:100

THE PROPOSAL

ensures that each flat therefore offers its occupants adequate levels/amounts of natural light, perceived space, privacy, ventilation and noise attenuation, securing adequate living conditions.

Appearance

8.19 The external alterations proposed are considered wholly in keeping with the host building and sympathetic of the character and appearance of the conservation area in which it is situated. External alterations comprise of the following:

- The installation of conservation-style rooflights; 2no on the rear gable projections' roof slopes, as well as 2no replacement rooflights to the rear roof slope of the host dwelling. The 1no. Rooflight on the front elevation will be removed.
- Renew the roof of the host dwelling with like-for-like replacements. A roof tile specification accompanies this submission.
- Subject to the roof replacement being approved and undertaken, this will necessitate the need to restore or re-build the chimney on the host dwelling which is visible from Cathedral Road. This will be either re-stored or re-built, to a like-for-like extent, replicating its existing visual appearance.
- Remove the rear chimneys at 164-166 Cathedral Road in full subject to agreement with owner of no. 164. If agreement is not confirmed with no.164, it is proposed to remove 50% of these rear chimneys (positioned on 166 Cathedral Road) and make good of the chimneys at no. 164 where necessary.
- Replace windows indicated pale blue on the 'Window details' plan in purpose made Tricoya rot proof timber sash windows, stained white.
- Replace windows coloured green on the 'Window details' plan like-for-like due to their deterioration over time.
- As aforementioned, extended rear roof dormers are also proposed. Dormer end elevations would comprise of slates as proposed on the roof of the host dwelling, whilst inward looking walls of the dormers will be epoxy resin render (white) on cement board. Bay windows are proposed from these dormers with front facing elevations to be of obscure glazing whilst side facing elevations remain clear.

8.20 The extended roof dormers are subservient to the host building and given their concealed location at the rear of the property within the confines of the existing host, they would not appear highly visible. Other external alterations are minor in scale and would largely be like-for-like replacements to ensure the character and appearance of the host building is preserved, and enhanced. An assessment of each of the above listed alterations/proposals is provided in the planning appraisal below.

8.21 Materials are proposed to accord with the existing material palette. The palette has been consciously selected to deliver an aesthetically attractive scheme whilst ensuring it assimilates with neighbouring properties and respects the character and appearance of the host building and the Conservation Area.

8.22 The proposed single storey extension is located at the rear of the site, meaning it would only be seen from the rear service lane which is not a significant contributor of the Conservation Area. Its position to the rear ensures it is not highly visible from many public vantage points and it should be reiterated that such extensions are common additions to the rear of such properties along Cathedral Road. In any case, the extension would not extend any deeper or higher than the rear extension approved as part of the original permission which was deemed acceptable. In comparison to this permission, the proposal now seeks to infill the approved rear courtyard space to form a new single storey rear extension. As such, when viewed from the rear its visual appearance would be no different to that originally approved.

Landscaping and Biodiversity

8.23 The site is currently characterised largely by expanses of concrete hard standing. It is proposed to formally re-landscape this area, upgraded with new paths and replacement hard and soft landscaping as shown on the proposed plans. This, along with the sedum roof as approved on the extension of the previous approval and the implementation of 2no. bird boxes at the rear of the property, will satisfy the requirements of the updated Chapter 6 of PPW, now contained within the 12th edition of PPW. This would be proportionate to the scale of the development proposed. Please refer to supporting Green Infrastructure statement for further detail.

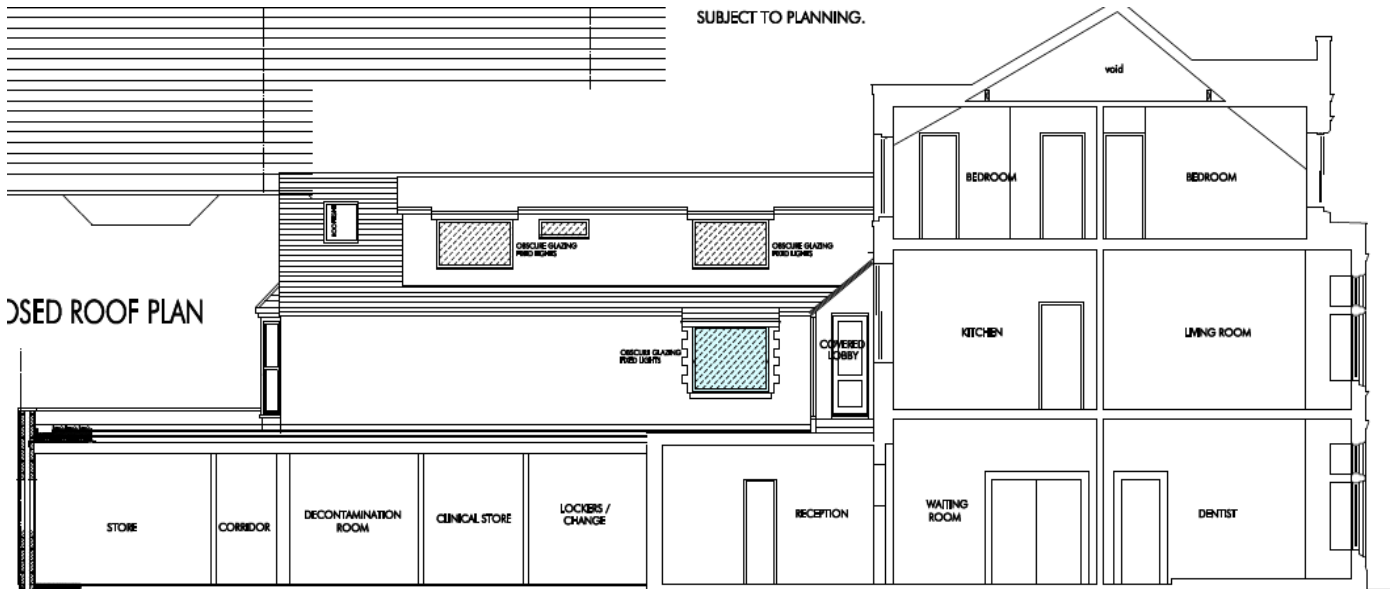
Community Safety

8.24 TAN 12 suggests that community safety can be achieved via design solutions that can aid crime prevention. These include providing natural surveillance, improving safety by reducing conflicts in uses, and promoting a sense of ownership and responsibility. Two separate access points are proposed for the separate uses, with the residential use accessed via 168 Cathedral Road and the dental survey accessed via 166 Cathedral Road, thus ensuring that the proposed uses are disconnected. Furthermore, CCTV is implemented on site to ensure that both residents and visitors of the dental clinic feel safe.

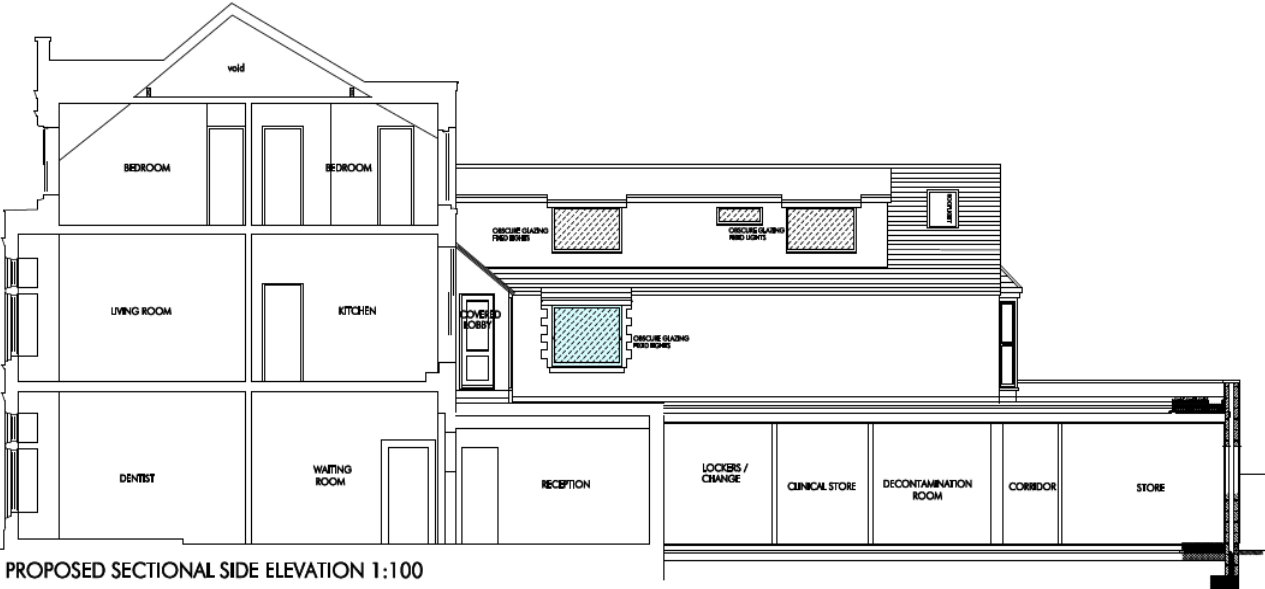
Environmental Sustainability

8.25 As identified within Technical Advice Note 12, the objectives that developers need to respond to in respect of Environmental Sustainability include that of achieving efficient use and protection of natural resources, enhancing biodiversity and designing for change. The site will be developed to accord with the latest Building Regulations standards. The overall sustainability principles of the development are therefore considered to be high.

PROPOSED SECTIONAL PLANS



PROPOSED ROOF PLAN



PROPOSED SECTIONAL SIDE ELEVATION 1:100

PLANNING, DESIGN & ACCESS STATEMENT

PLANNING APPRAISAL

Planning Appraisal

9.1 As stated above, Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that planning applications be determined in accordance with the Development Plan unless material considerations indicate otherwise. This section of the Planning Statement aims to identify the main issues of relevance to the determination of the Planning Application, and assess the scheme against the Development Plan.

9.2 The key material considerations of relevance in the determination of the application are as follows:

- The principle of development at this location;
- The impact of the development on the character and appearance of the surrounding area;
- The impact of the development in terms of residential amenity;
- The impact of the development in terms of highways and access; and
- Other material considerations;

Principle of Development

9.3 The proposed development is relatively minor in scale, with external alterations along with extended rear dormers proposed to accommodate internal alterations to create enjoyable and habitable spaces for future occupiers of the flats on first and second floors. The covered lobby at first floor and the principle of the ground floor rear extension has already been deemed acceptable as part of the original permission, with the external appearance of the rear extension appearing no different to what was initially approved. The site is located within the settlement boundary, as such is located within an area where development is normally permitted.

9.4 The site brings improved housing to a highly sustainable location, helping to contribute towards housing delivery in the city of Cardiff.

9.5 The principle of development is therefore established in accordance with LDP policies KP1 (Level of Growth) and KP3 (B) (Settlement Boundaries), as well as Policy 1 (Where Wales

will grow) of Future Wales.

Character and Appearance

9.6 As discussed in section 8, it is considered that the proposals are sympathetic of the existing character and appearance of the building and Conservation Area. As aforementioned, an assessment of each of each of the elements proposed is provided below:

Rooflights

9.7 It is proposed to remove the front rooflight currently positioned on no. 166 Cathedral Road. This would be an improvement to the visual appearance of the property and its contribution to the Conservation Area given the prominence of this elevation along Cathedral Road.

9.8 The 2no rooflights proposed to the rear gable end projection of 166-168 Cathedral Road would face inward and would be contained within the existing built form. Given this and their position to the rear of the site, they would not be highly visible from important vantage points within the conservation area with only a rear access lane running immediately to the rear of the site. Accordingly, the Cathedral Road Conservation Area Appraisal states that rooflights (and dormer windows) are more likely to be considered acceptable on rear elevations as they tend to be discrete locations. Notwithstanding, the rooflights would be conservation style, flush with the existing roof plane, ensuring they relate sympathetically to the character of the conservation area.

9.9 The additional 2no rooflights which are to replace those currently in situ on the rear of 166-168 Cathedral Road would be like-for-like replacements and as such, would not alter the existing appearance of the building.

Roof replacement and chimney removals/restoration

9.10 These roof replacement works are necessitated to ensure its structural integrity is secured, and that the building can continue to serve its purpose for years to come. Such roof works proposed will therefore secure the longevity of the building. The extent of the roof replacement will be determined following inspection. Notwithstanding, roof tile replacements will be like-for-like to ensure the impact upon the character and appearance of the conservation area is maintained and enhanced. Product details of the proposed slates accompany this application.

9.11 Depending on the extent of roof work required, restoration or rebuilding works to the host building chimney will need to be undertaken. The extent of such works will be determined upon inspection of the roof. Again, any such works will be like-for-like to preserve the existing appearance of the property when viewed from Cathedral Road.

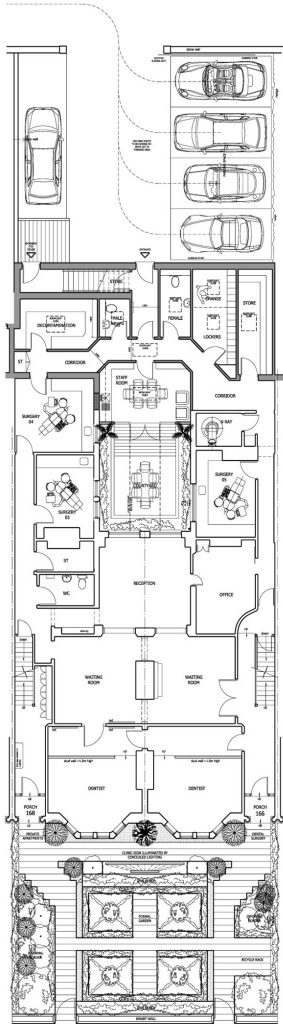
9.12 As aforementioned, it is also proposed to remove the rear chimneys at 164-166 Cathedral Road visible from the rear access lane. The extent of their removal will depend upon discussions with the neighbour at no. 164 and a subsequent Party Wall agreement. The Cathedral Road Conservation Area Appraisal states that the removal of a chimney which makes a contribution to the conservation area will not normally be supported. This rear chimney proposed to be removed is not highly visible from the main thoroughfare that is Cathedral Road and is only viewed from the rear access lane in a concealed and largely secrete location. As such, it is not prominent from the highway (excluding rear service lanes) or from significant vantage points which contribute to the character of the Conservation Area.

9.13 Furthermore, chimneys located to the rear of properties along this particular stretch of Cathedral Road have already been removed and as such, they are not a common or consistent feature which characterises these properties within the Conservation Area. Therefore, it is not considered to be a feature which makes a strong contribution to the conservation area and its loss would therefore not be significant or detrimental.

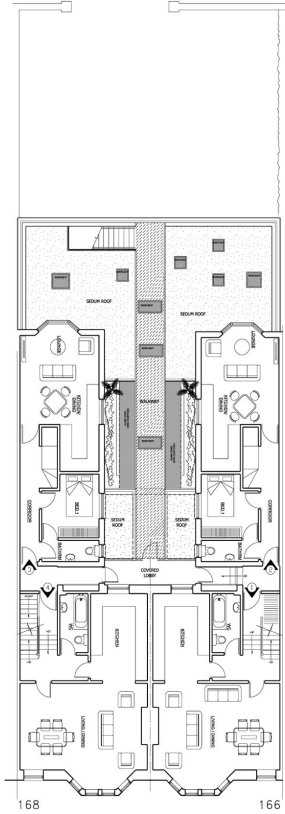
Replacement Windows

9.14 It is proposed to replace windows located at both the front and rear elevation of 166-168 Cathedral Road. Those coloured blue on the 'window details' plan will be replaced with Tricoya rot proof timber sash windows, stained white. Those coloured green will be replaced exact like-for-like, which are fixed and non-opening to comply with regulations brought in during Covid-19. This is to secure longevity for the building given the deterioration of the windows over time and it will secure acceptable building standards. The Cathedral Road Conservation Area Appraisal states that window replacements shall carefully match the appearance, materials and function of the original windows. The ground floor windows on the front elevation will replicate the existing appearance of the windows on this front façade. The proposed Tricoya timber sash windows elsewhere on the property are considered appropriate replications of the existing windows in situ on the building, and

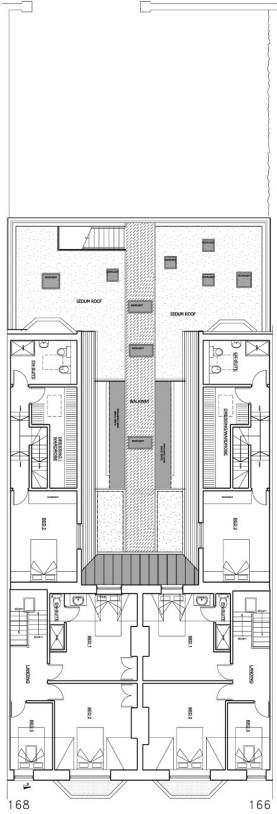
PROPOSED FLOOR PLANS



PROPOSED GROUND FLOOR PLAN



PROPOSED FIRST FLOOR PLAN



PROPOSED SECOND FLOOR PLAN

NOT TO SCALE - PLEASE REFER TO SUBMITTED DRAWINGS

PLANNING APPRAISAL

sympathetic of the Conservation Area and neighbouring properties. The exact manufacturer's specification will be confirmed at a later date once the client puts the project out for tender and receives appropriate options from different companies. It is sought to secure this exact detail via condition.

Rear roof dormer windows

9.15 Dormer windows located to the rear of properties along this stretch of Cathedral Road are common additions. A number of properties within the surroundings of the application site have also been extended, including the provision of rear dormers (both flat and pitched roof style). Rear roof dormers at the application site have already been deemed acceptable by virtue of the previous permission (ref. 19/02271/MNR) which permitted two dormers sited on the side roof slopes of the rear annexe, measuring 2.5m deep, 7m wide and 2.5m high with a flat roof. The dormers as now proposed would be positioned on the same two side roof slopes, within the confines of the existing built form. Whilst the proposed new dormers would be slightly greater in length, their location to the rear and on the side roof slopes ensures they would not be highly visible nor prominent from significant or important vantage points within the Conservation Area. They would still be contained within the existing built form and set back from the eaves and the gable projection at its furthest end, which ensures their subordination to the host and that its relationship with the existing property and that of neighbouring properties is acceptable.

9.16 The dormer end elevations would comprise of slates as proposed on the roof of the host dwelling (please see supporting 'Roof Tiles Specification'). The inward looking walls of the dormers will be epoxy resin render (white) on cement board. Proposed materials are therefore considered sympathetic of the local vernacular.

9.17 As aforementioned, the covered lobby at first floor has already been considered and deemed acceptable as part of planning application ref. 19/02271/MNR. The ground floor single storey rear extension would project no deeper, nor would it extend any higher than the rear extension approved as part of the previous approval. Whilst it is now proposed to infill the previously approved courtyard space, this would have no bearing on the external visual appearance of the extension, compared to that approved. The sedum roof as originally approved

would also be implemented as part of the now proposed scheme, retaining this accepted feature. As such, these elements of the scheme would not deviate from the fallback position.

9.18 Given the above assessment undertaken against each of the proposed elements, it is concluded that the proposed works would not be harmful to the visual amenity of the Conservation Area and are of an acceptable scale and appearance that would ensure compliance with LDP policies KP5 (Good Quality and Sustainable Design), KP17 (Built Heritage) and EN9 (Conservation of the Historic Environment).

Residential Amenity

9.19 The impact of the proposal on residential amenity and privacy has been carefully considered. As previously discussed, the internal floorspace of each flat meets the requirements and space standards set out within the SPG. A sufficient number of window openings are proposed to ensure adequate ventilation and access to light for future occupiers.

9.20 It is acknowledged that habitable bedroom windows face one another on both the 1st and 2nd floors of 166-168 Cathedral Road. However, direct facing elevations of such windows would be fitted with obscure glazing to remove any overlooking concern. These windows are projecting bay windows with the side facing elevations remaining clear, to allow adequate outlook for future occupiers of said bedrooms. As such, it is considered that the proposal would not prejudice the privacy or outlook, and therefore the living conditions of future occupiers.

9.21 Whilst it is acknowledged that there is a lack of amenity space for future occupiers of the apartments, the site's long-established residential use on first- and second-floor, proximity to extensive parkland, as well as adequate provision of cycle parking, ensures that future occupiers are not disadvantaged in this respect.

9.22 In light of the above, it is considered that the proposed development would not have an adverse impact upon the residential amenity of existing properties, and the amenity of future occupiers will be secured. Therefore, the scheme adheres to Policy H5 (Sub-division or Conversion of

Residential Properties) and KP5 (Good Quality and Sustainable Design) of the LDP.

Highways and Access

9.23 As discussed under section 8, vehicular and pedestrian access into the site is established and will be retained as part of the proposals. The site lies within the Central Area where there is no requirement for parking provision to serve residential uses.

9.24 The site lies in close proximity to sustainable modes of transport, and the provision of cycle parking facilities will further encourage future occupiers to travel to and from the site by bike. The site's sustainability credentials cannot be argued with.

9.25 Given the above, occupiers of the apartments would have access to a wide range of sustainable and active means of transport, contributing to the ambitions of national planning policy to reduce reliance on the private motor vehicle. This is in line with the ambitions of Future Wales and PPW, and LDP policies KP8 (Sustainable Transport) and T1 (Walking and Cycling).

Other Material Considerations

Waste

9.26 Adequate waste storage facilities are provided to the rear of the property and out of sight of the main thoroughfare of Cathedral Road. Waste will be presented onto Dogo Street on collection day, ensuring appropriate waste management arrangements are in place to serve the occupiers of the proposed flats. Notwithstanding, the proposal does not result in a greater number of apartments. In this respect, the proposal complies with LDP Policy W2 (Provision for Waste Management Facilities in Development).

Ecology and Biodiversity

9.27 The proposals, due to their scale, are not anticipated to cause detrimental impacts to biodiversity on-site. The proposal will put forward a re-landscaped front garden and the implementation of 2no. Bird boxes to help meet biodiversity net gain. This is supported by the accompanying Green Infrastructure Statement. The proposal therefore complies with LDP policy KP16 (Green Infrastructure).



CONCLUSION

10.1 This Planning, Design and Access Statement (PDAS) has been prepared on behalf of Mr Ali Tabai to accompany the full planning application for a single storey rear extension at ground floor and covered rear lobby on the first floor, proposed extension to rear roof dormers, external alterations including replacement windows, re-roofing, re-build host property chimney, removal of rear chimneys, internal alterations and associated works at 166-168 Cathedral Road. The removal of the rear chimneys at 164 Cathedral Road is also proposed, subject to agreement with the owner.

10.2 The amount, scale, layout and appearance of the proposal have been discussed throughout this document and are considered to be in keeping of the surrounding context. Proposals ensure the most sustainable and considered use of the site. Proposals for the residential spaces ensure that resident amenity and enjoyment of the dwelling is of paramount importance.

10.3 In view of the above, it is considered that the proposals have suitably addressed the key material considerations for this site as discussed above, and its approval would provide a quality development that would complement and enhance what currently exists.

10.4 Consequently, it is considered that this Planning, Design and Access Statement clearly demonstrates that the scheme, as presented, complies with the relevant National and Local Planning Policies. Furthermore, close attention has been given to the site's location within the Cathedral Road Conservation Area and an understanding and appreciation of its special character.

10.5 As such it is respectfully requested that Cardiff Council grant full planning consent for the proposals at 164-168 Cathedral Road, Cardiff.